



Public Meeting

June 25, 2024

**Wells Route 1 Corridor
Transportation Feasibility Study**



Approx.
5.6 miles

Public Meeting Agenda – June 25, 2024

- Welcome and Introductions
- Project Background
- Purpose and Need
- Study Process and Timeline to Date
- Existing Conditions and High Crash Locations
- Summary Overview of Draft Recommendations
- Public Input
- Next Steps

Project Overview

- **Planning Partnership Initiative (PPI):** Town of Wells in collaboration with the Maine Department of Transportation (MaineDOT).
- **Generational Opportunity:** The Town reached out to MaineDOT to inquire about a comprehensive study to help evaluate potential improvements. Route 1 in Wells has not had this type of data driven, solutions-oriented study completed in the past.
- **Location:** The study area focuses on Route 1 beginning at the Wells/Ogunquit Town line and proceeds north for approximately 5.6 miles to the intersection of Route 9/Bypass Road.



Study Purpose

Study's purpose is to *identify transportation and land use recommendations to improve safety, efficiency, and mobility for all transportation modes, primarily within the existing ROW, while complimenting economic development. The study will evaluate highway safety and mobility but also emphasize improvements for Active Transportation and Transit.*



Study Process – Overview

- Data collection, summary of background information and ongoing projects, and evaluation of existing conditions
- Review area crashes, specifically at High Crash Locations (HCL)
- Performed Road Safety Audit (RSA)/field review
- Develop safety and mobility improvements (short-term and long-term)



Timeline to Date

- 10/19/22 – 6/25/24 Project Kickoff/TAC Meetings (6)
- 11/8/22 – 11/9/22 Conducted Road Safety Assessment (RSA)
- 12/14/22 Public Meeting #1
- 6/6/23 Board of Selectmen Meeting
- 9/14/23 Field Review
- 11/15/23 Stakeholder Meeting
- 4/1/24 MaineDOT/Town Technical Meeting
- 6/25/24 Open House and Public Meeting #2
- Summer 2024 – Finalize Draft/Final Report

Technical Advisory Committee Members

Member

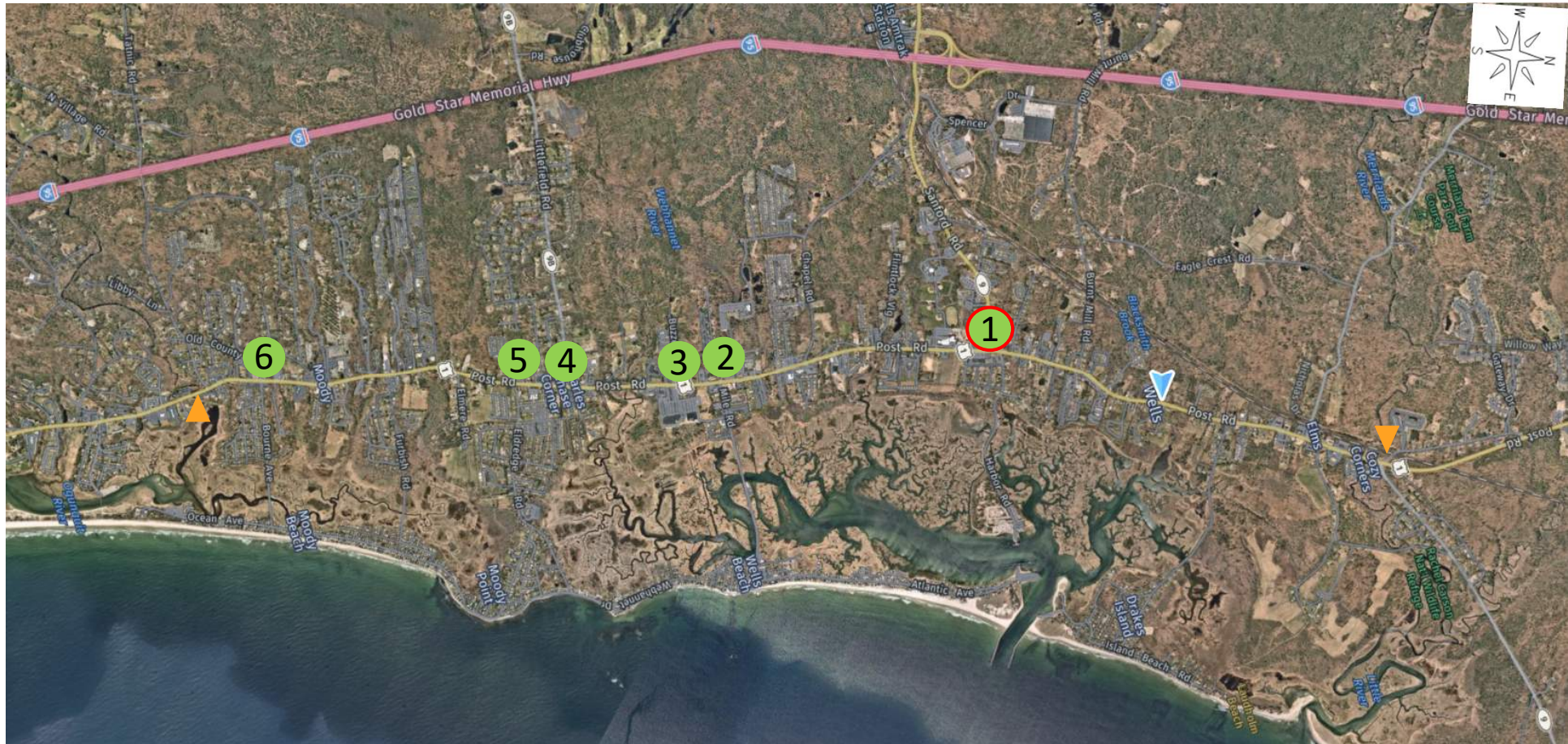
1. Michael Pardue
2. Carol Murray
3. Michael Livingston
4. Chris Baez
5. Marty Rooney
6. Dean Williams
7. Tony Grande
8. Jason Ready

Affiliation

- Town of Wells, Town Manager
- Town of Wells, PW Director
- Town of Wells, Planner/Engineer
- Town of Wells, Police
- MaineDOT, Planning
- SMPDC, Planner
- VHB, Project Manager
- VHB, Traffic

- ✓ Advisory role to the Study Team
- ✓ Provide input, guidance and direction at each step in the process

Wells Route 1 PPI Study Area (~5.6 miles) and Focus Areas



Focus Intersections along Route 1:

1. Sanford Rd (Rt 109)
2. Mile Rd
3. Steeple Way/Hannaford Plaza
4. Littlefield Rd (Rt 9B)
5. Eldridge Rd
6. Bourne Ave

WELLS RSA TRAFFIC VOLUME



AADT

- 0 - 200
- 200 - 500
- 500 - 1,000
- 1,000 - 1,500
- 1,500 - 2,500
- 2,500 - 4,000
- 4,000 - 7,000
- 7,000 - 12,000
- 12,000 - 17,000
- 17,000 - 100,000

Summer Peak = ~20,000 AADT
Off Peak = ~7,300 AADT

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2.5 Miles
 1 inch = 1.8 miles

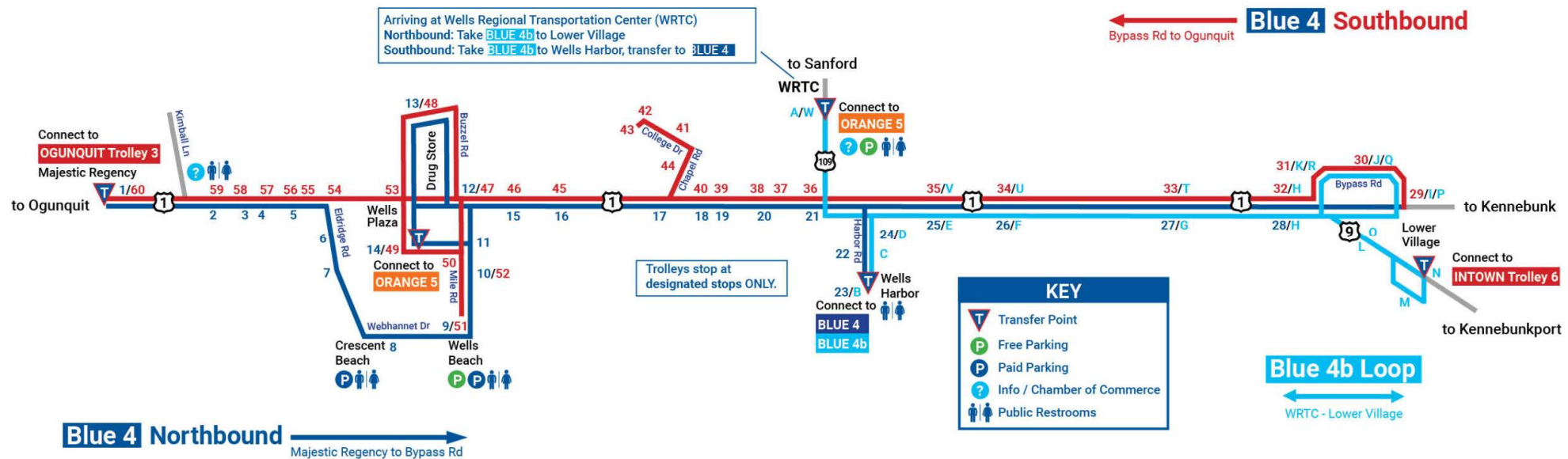
**AADT data from
 Wells/Ogunquit town line
 January/July 2022**

Date: 11/4/2022
 Time: 4:38:33 PM



Existing Conditions

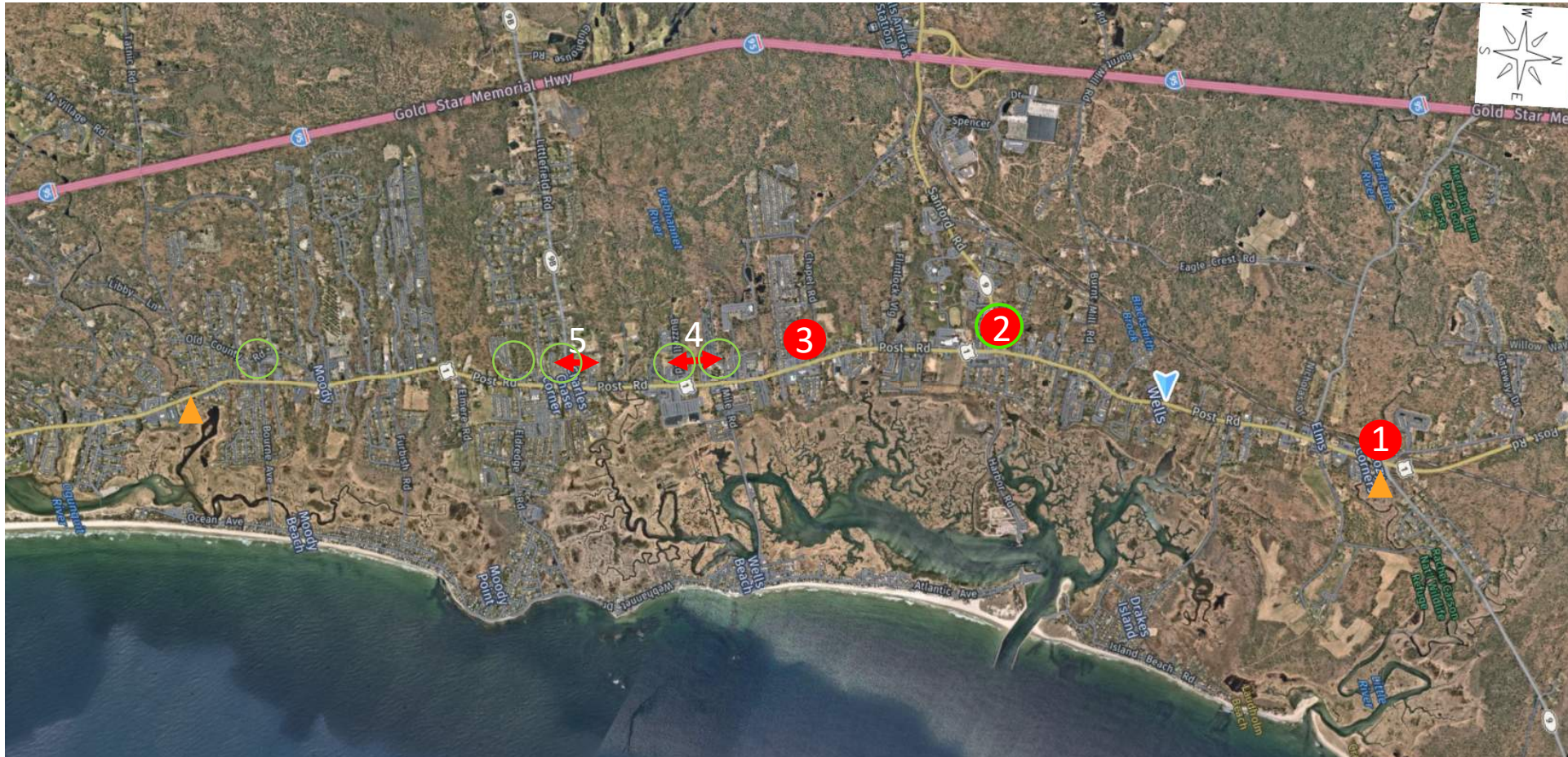
- Route 1 is very busy during beach season then activity drops off
- Generally, three lanes with a continuous two-way left-turn lane
- Overhead Utilities included throughout, but limited street lighting
- Underground Utilities – water, sewer, gas, fiber optic
- Comprehensive Plan – Update completed in May 2024
- Transit service (Orange) with limited/seasonal options (Shoreline Explorer)



Existing Conditions

- Driveways are dense throughout corridor
- Sidewalk throughout Study Area, but inconsistent width/material/length (Town Sidewalk Plan 2003)
- Limited ADA compliant accommodations
- Intermittent pedestrian crossing locations along the corridor
- Bicyclists and pedestrians, both use shoulders and sidewalks
- Posted Speed Limits 35-40 mph
- Five total High Crash Locations (HCLs) on Route 1 (2019-2021)

High Crash Locations and RSA Focus Areas



Route 1 High Crash Locations (2019-2021):

1. Port Rd/Rt 9
2. Harbor Rd
3. Chapel Rd

4. Segment from Mile Rd to Buzzell Rd
5. Segment from Littlefield Rd to Brown Ln

High Crash Locations

- MaineDOT uses two criteria to define a High Crash Location (HCL). Both criteria must be met in order to be classified as an HCL.
 1. A critical rate factor (CRF) of 1.00 or more for a three-year period. A CRF compares the actual crash rate to the rate for similar intersections in the state. A CRF of less than 1.00 indicated a rate of less than average and:
 2. A minimum of eight crashes over the same three-year period

- Total Crashes (2019-2021): **57 total crashes**
 1. Route 1 and Post Road/Route 9 (Intersection): 8 Crashes, CRF 1.83
 2. Route 1 and Harbor Road (Intersection): 8 Crashes, CRF 1.54
 3. Route 1 and Chapel Road (Intersection): 14 Crashes, CRF 2.23
 4. Route 1 (Segment) from Littlefield Rd to Brown Ln: 9 Crashes, CRF 1.25
 5. Route 1 (Segment) from Mile Rd to Buzzell Rd: 18 Crashes, CRF 1.53

Locations Close to HCL Status / Areas to Watch

Total Crashes (2019-2021):

- A. ****Route 1 and Hannaford Plaza (Intersection):**
13 Crashes, CRF 0.62
- B. Route 1 and South Street (Intersection):
Residential area with schools
- C. Route 1 (Segment) from Mile Road to Bayview Terr:
(Continuation of existing HCL)
- D. Route 1 (Segment) from South Street to Bayley Road:
(Residential area with School)
- E. Route 1 (Segment) from Furbish Road to Elmere Road:
8 Crashes, CRF 0.46

**Focus Area Intersection

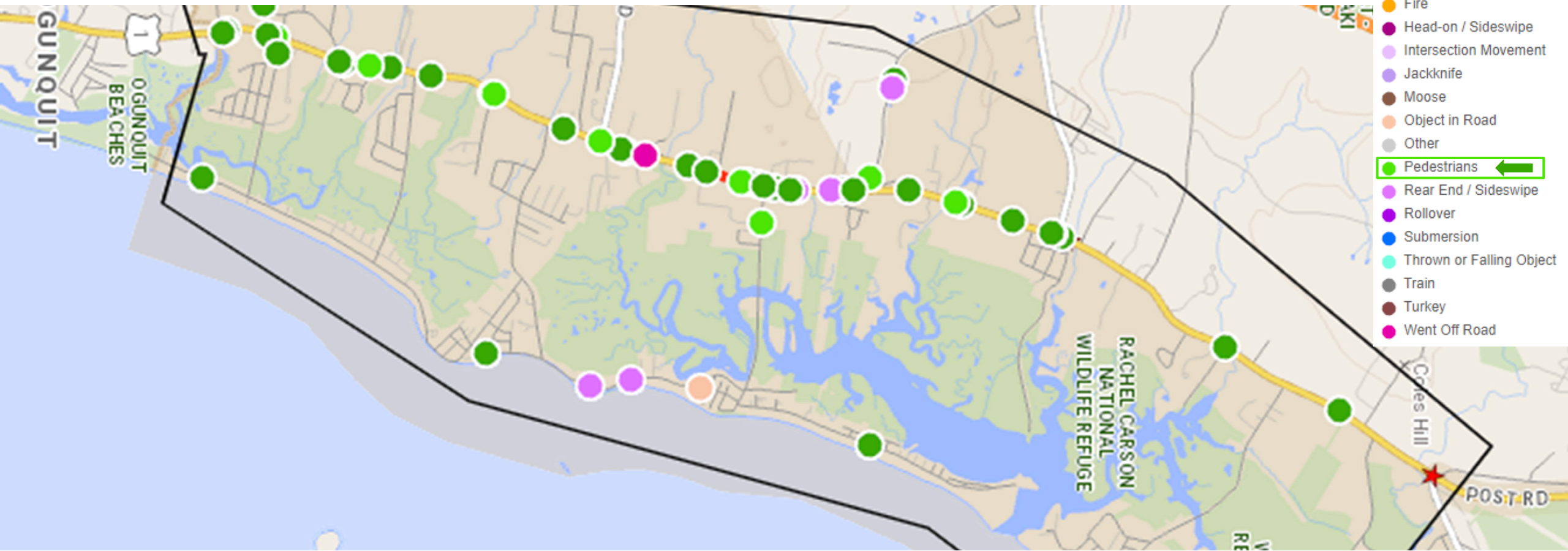


10 Year Bike and Pedestrian Crashes



Crashes - 10 Years

- × <Null>
- All Other Animal
- Bear
- Bicycle
- Deer
- Fire
- Head-on / Sideswipe
- Intersection Movement
- Jackknife
- Moose
- Object in Road
- Other
- Pedestrians
- Rear End / Sideswipe
- Rollover
- Submersion
- Thrown or Falling Object
- Train
- Turkey
- Went Off Road



47 Bicycle Crashes
14 Pedestrian Crashes
11th Highest Rate in the State

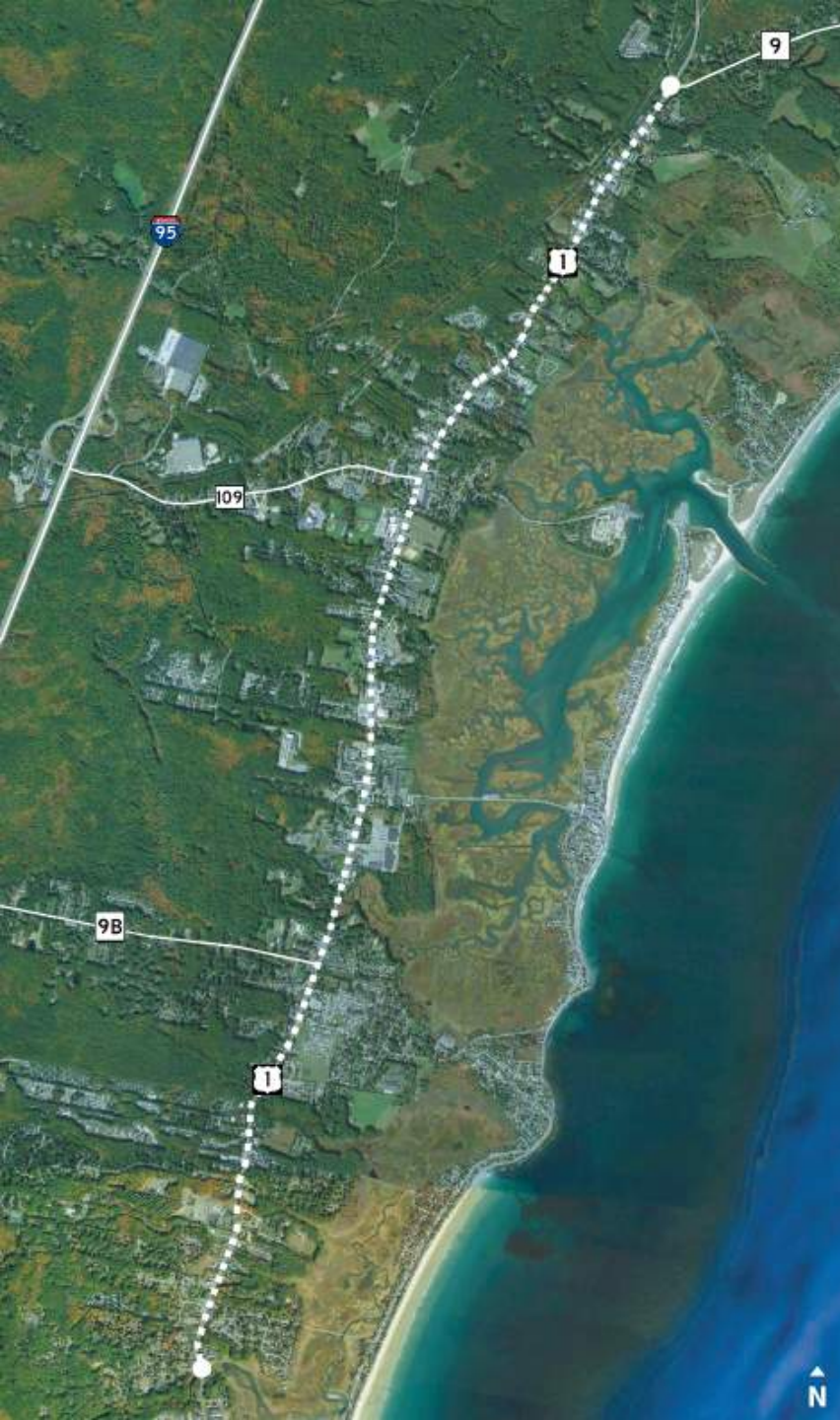


Summary of Recommendations

- **Improve active transportation opportunities throughout the corridor**
 - Promote consistent travel lane widths to 11' with 5' shoulders/bike lanes
 - Two-way center left-turn lanes will be 12' wide, where provided
 - Expanded sidewalks (*continuous sidewalk within study area, future WTC*)
 - Enhanced crossing locations along Route 1
 - Potential Expanded/Enhanced transit stops along the corridor
- **Traffic and Safety Improvements** (*no additional traffic lanes*)
 - New traffic signal technology with emergency preemption
 - Raised center medians in specific locations to mitigate crashes
 - New traffic configurations at selected intersections
- **Aesthetics and Lighting**
 - Gateway Treatments at each end of the project
 - Additional pedestrian lighting

Review of Draft Concept Plans

Wells Route 1 Corridor
Transportation Feasibility Study

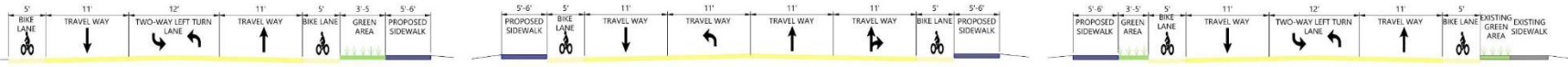




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TYPICAL CROSS-SECTION B-B

TYPICAL CROSS-SECTION C-C

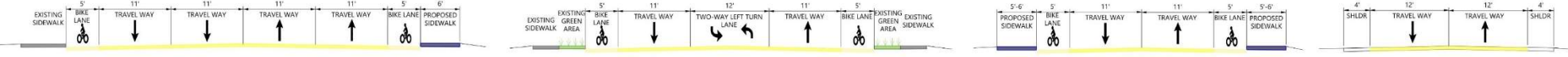


TYPICAL CROSS-SECTION D-D

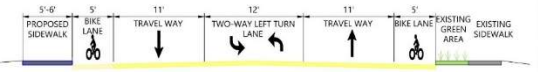
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TYPICAL CROSS-SECTION F-F

TYPICAL CROSS-SECTION G-G

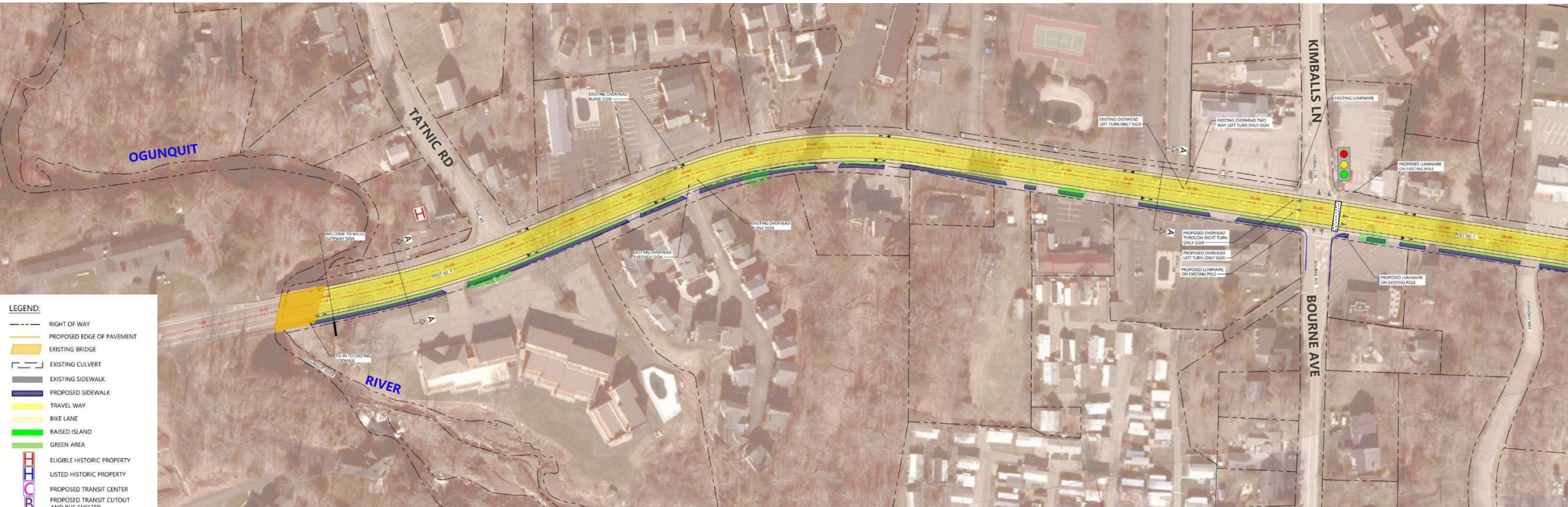
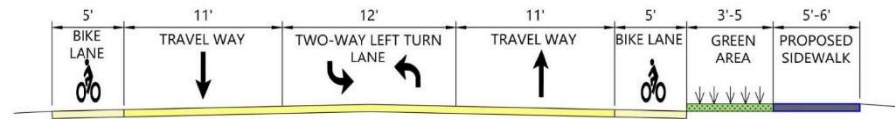


TYPICAL CROSS-SECTION H-H



- Consistent lane widths
- Traffic Signals 5 existing
2 new
- RRFBs 5 existing
10 new
- Sidewalks inconsistent
Add to Easterly side

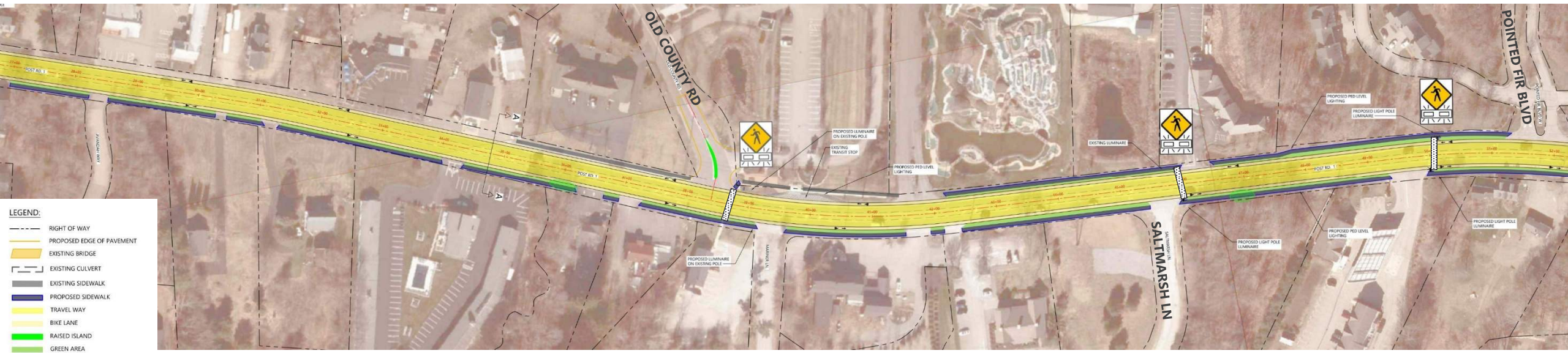
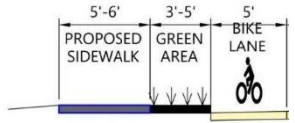
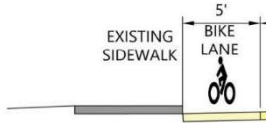
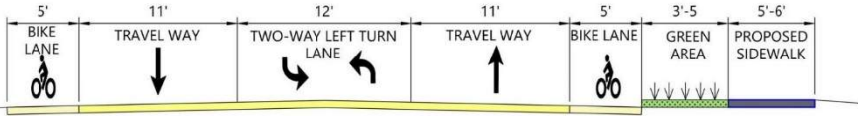
TYPICAL CROSS-SECTION A-A



- LEGEND:**
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 - EXISTING SIDEWALK
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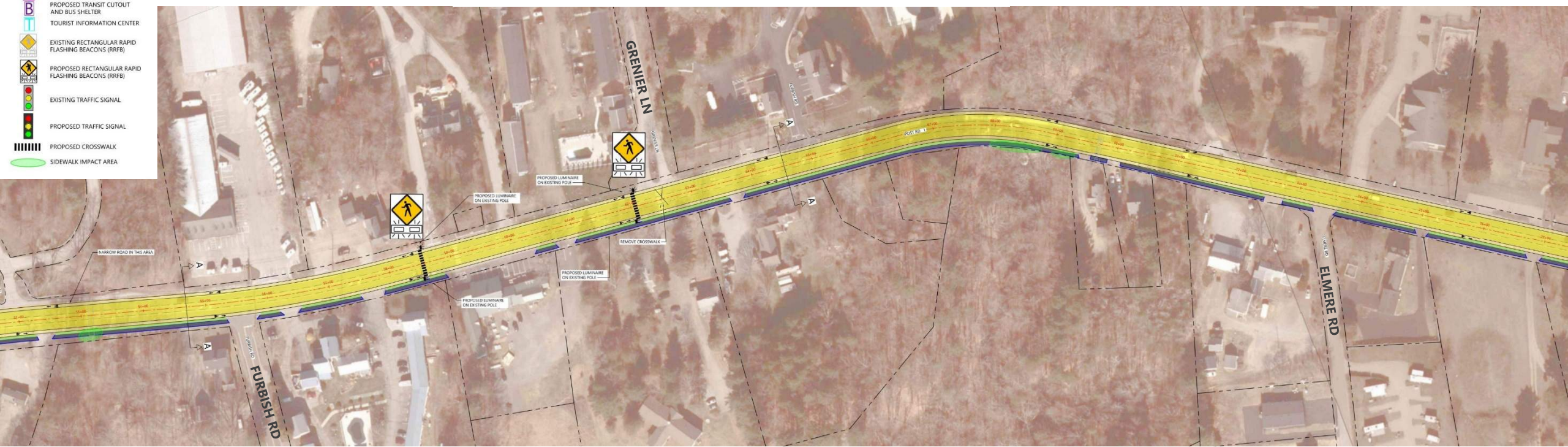
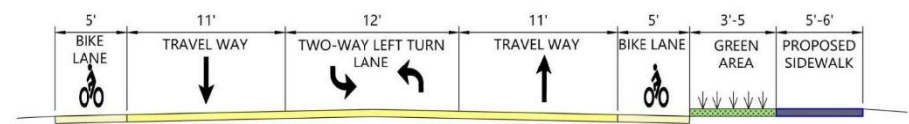
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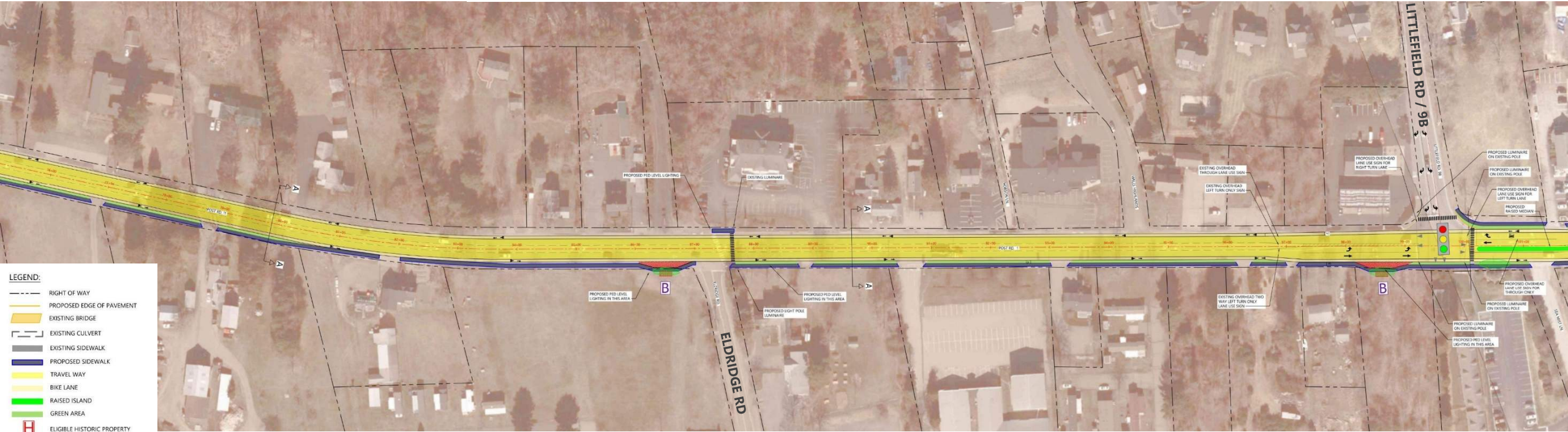
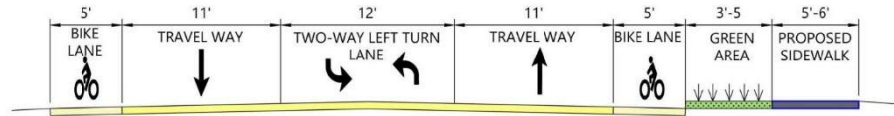
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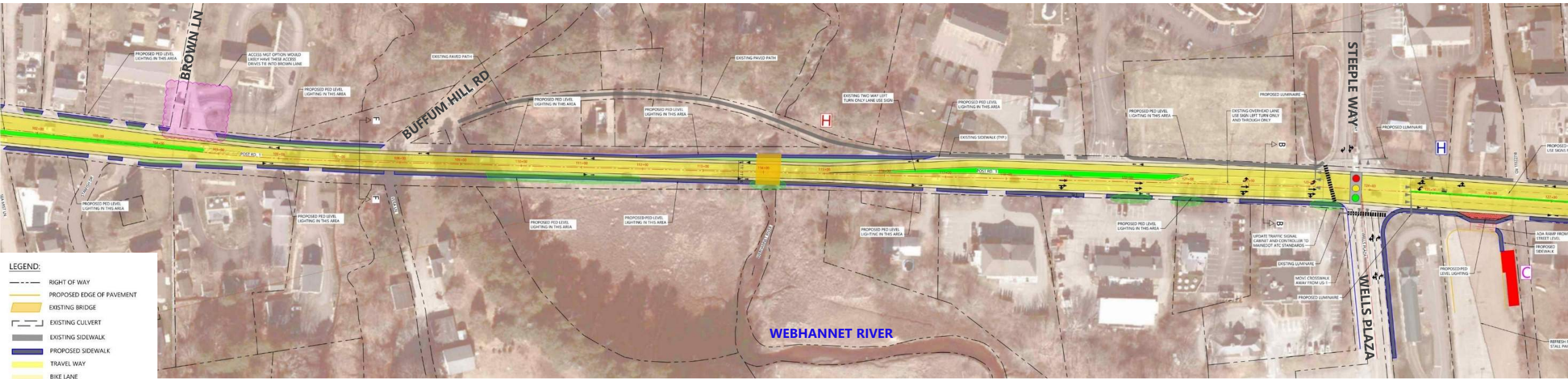
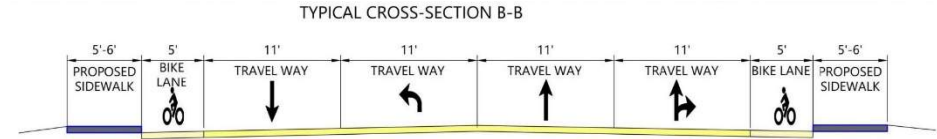
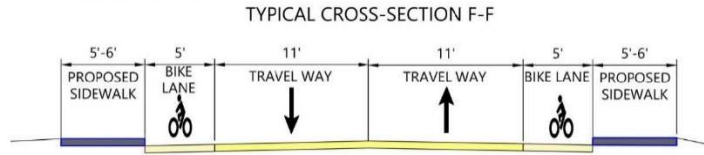
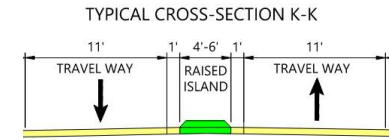
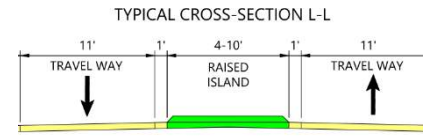
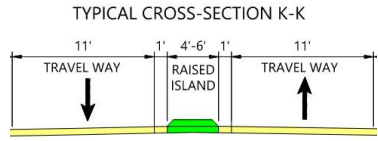


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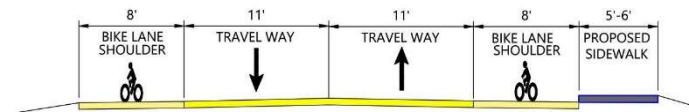


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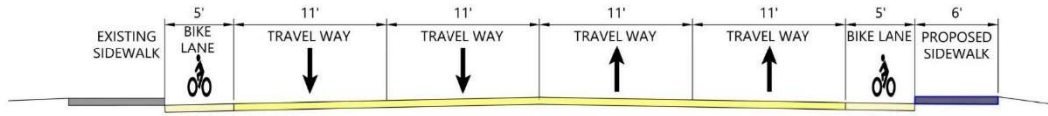




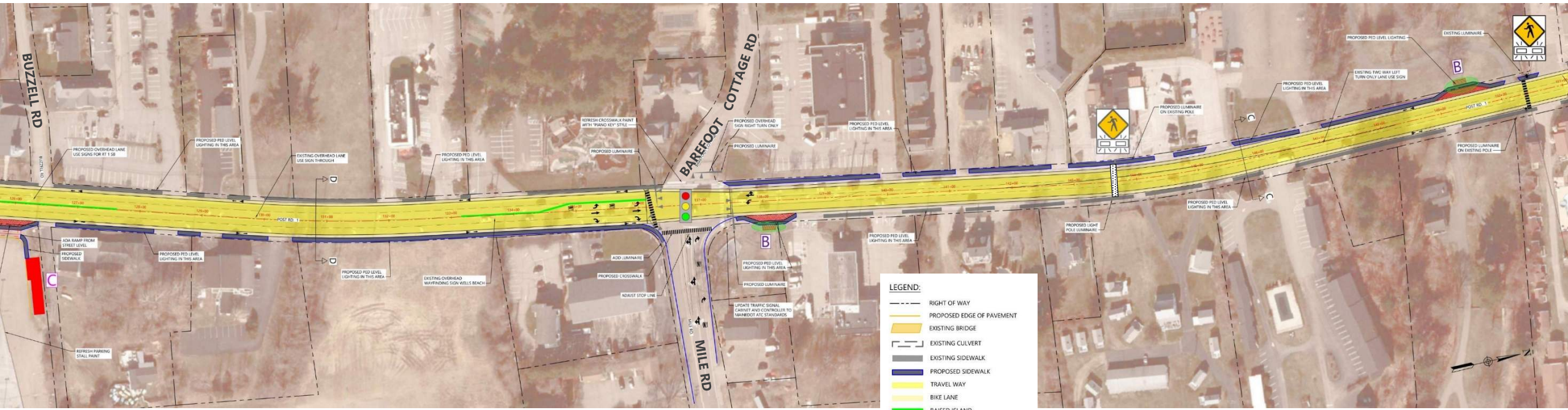
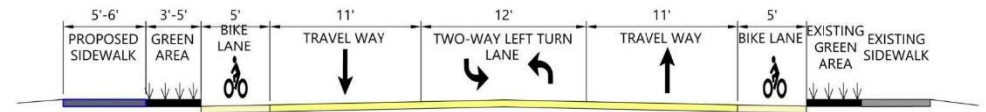
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TYPICAL CROSS-SECTION D-D



TYPICAL CROSS-SECTION C-C



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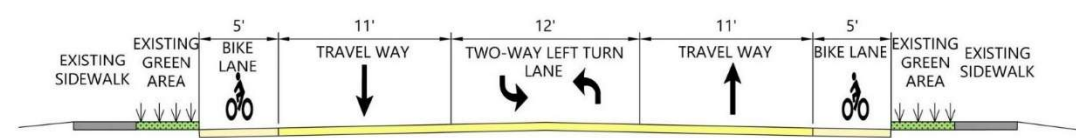
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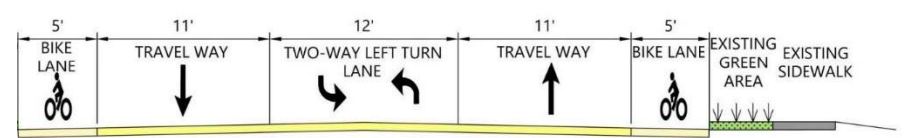
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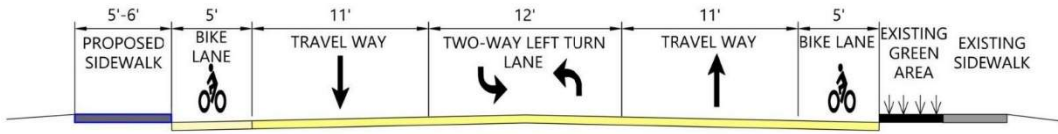
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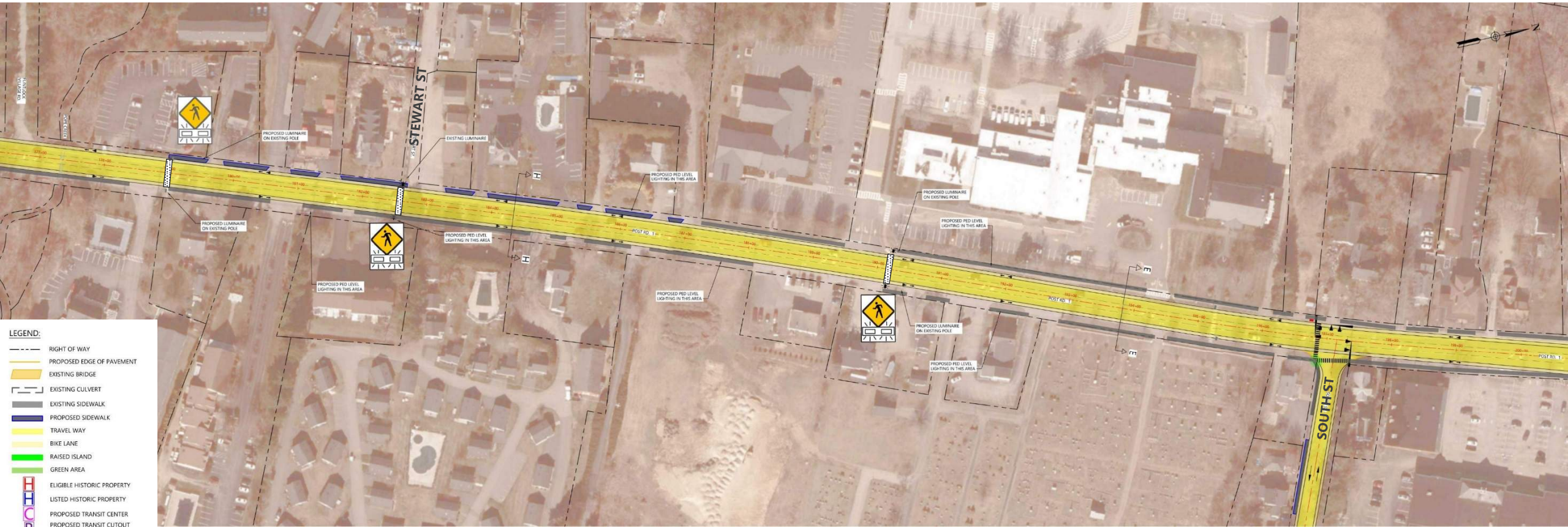
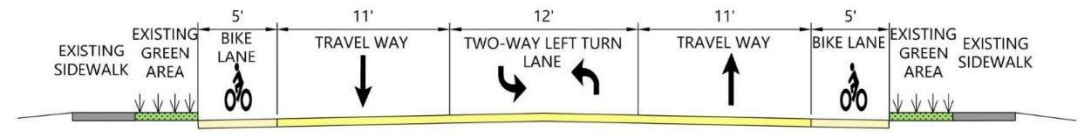
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TYPICAL CROSS-SECTION H-H



TYPICAL CROSS-SECTION E-E

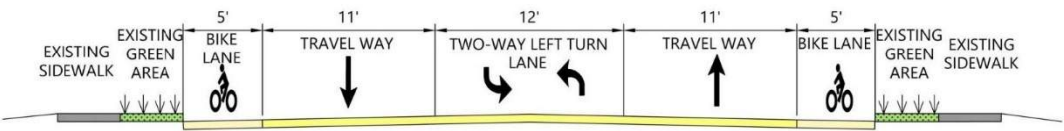


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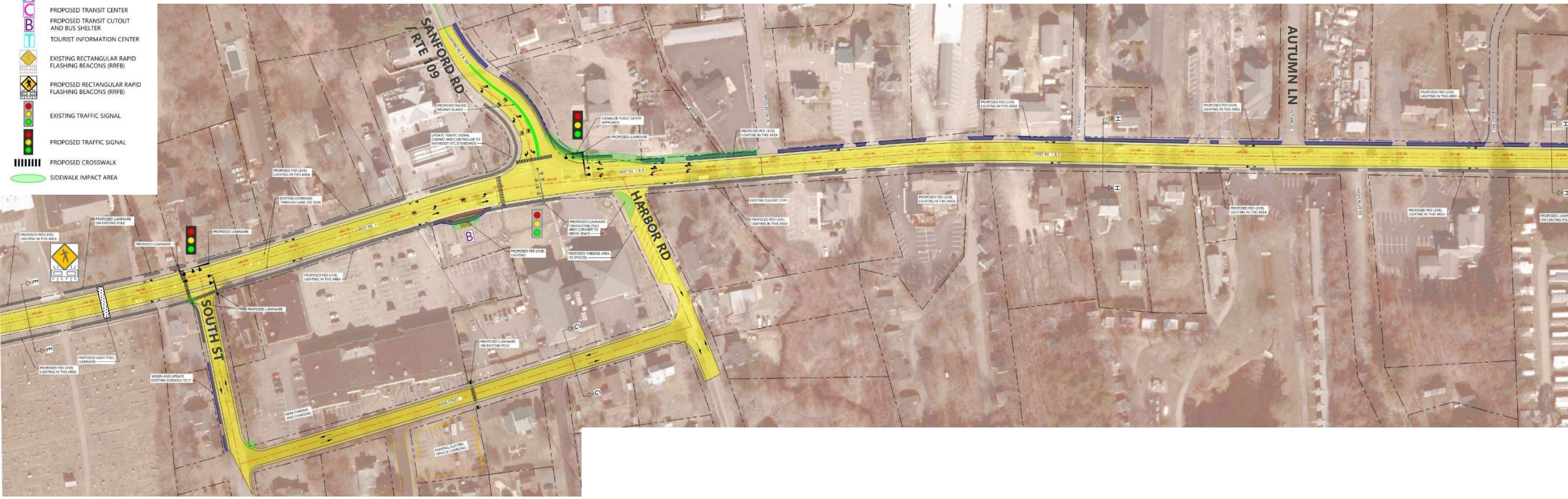
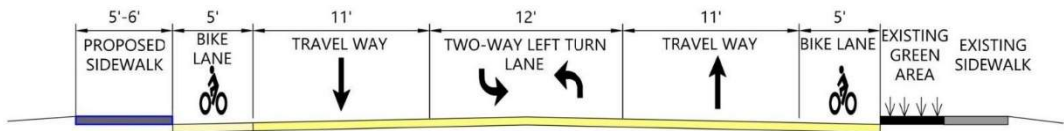


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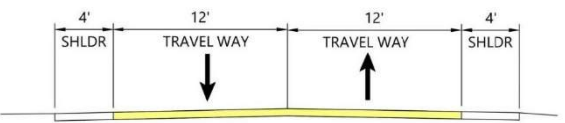
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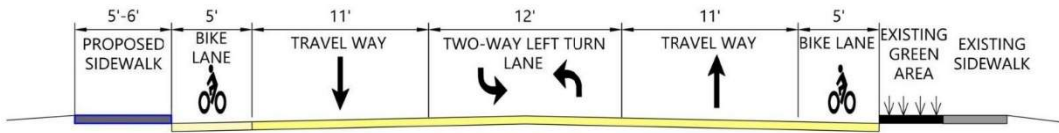
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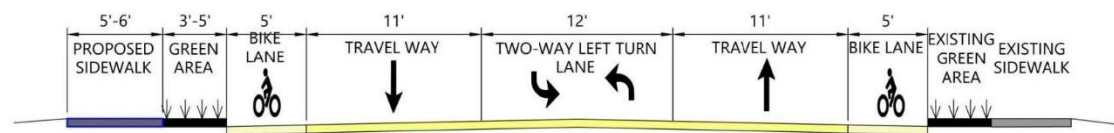
TYPICAL CROSS-SECTION G-G



TYPICAL CROSS-SECTION H-H



TYPICAL CROSS-SECTION C-C

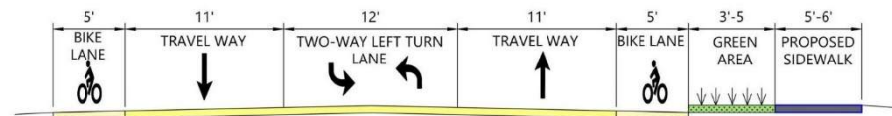


LEGEND:

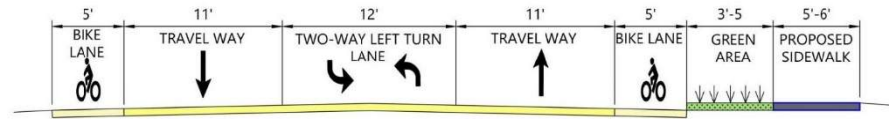
- RIGHT OF WAY
- PROPOSED EDGE OF PAVEMENT
- EXISTING BRIDGE
- EXISTING CULVERT
- EXISTING SIDEWALK
- PROPOSED SIDEWALK
- TRAVEL WAY
- BIKE LANE
- RAISED ISLAND
- GREEN AREA
- ELIGIBLE HISTORIC PROPERTY
- LISTED HISTORIC PROPERTY
- PROPOSED TRANSIT CENTER
- PROPOSED TRANSIT CUTOOUT AND BUS SHELTER
- TOURIST INFORMATION CENTER
- EXISTING RECTANGULAR RAPID FLASHING BEACONS (RRFB)
- PROPOSED RECTANGULAR RAPID FLASHING BEACONS (RRFB)
- EXISTING TRAFFIC SIGNAL
- PROPOSED TRAFFIC SIGNAL
- PROPOSED CROSSWALK
- SIDEWALK IMPACT AREA



TYPICAL CROSS-SECTION A-A

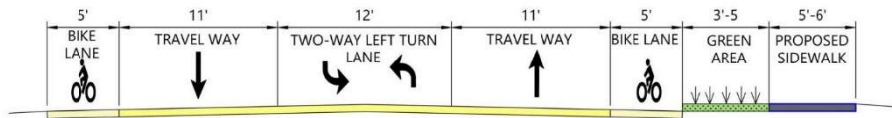


TYPICAL CROSS-SECTION A-A



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Potential Project Phasing

- **Ogunquit Town Line to Littlefield Road (~1.7 miles)**
- **Littlefield Road to Sanford Road (~2.0 miles)**
- **Sanford Road to Bypass Road (~1.8 miles)**
- **Chapel Road Sidewalk**
 - Route 1 to College Drive
 - College Drive to WTC

Public Input

- Questions or Comments??



Next Steps

Public Meeting Follow up:

- Collect and summarize all feedback and ideas from today's meetings
- Finalize recommended improvements

Proposed Project Schedule:

- Summer 2024 – Prepare Report

Additional Comments

- Additional Comments can be emailed to:
 - Rebekah Kelley at rkelley@wellstown.org
 - Tony Grande at agrande@vhb.com



We will be accepting comments through July 12, 2024

Thank You!!

