

VI. Transportation by SMPDC

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TRANSPORTATION

Introduction

Roads, streets, and other means of transportation are often referred to as the town's circulation system. This system is necessary to move people, goods, and services from one part of town to another, into town, out of town, and through town.

The highway system also provides access to private property. Roads may be thought of as the framework upon which the town is built. In addition to these functions the highway system is also the setting from which we view much of the town. The views from the roads in town - views of fields, forests, and ocean the places where people live and work - form the visual impressions of our community. The efficiency of our town, the value of our land, and how we view and experience our surroundings are all affected by the highway system and how well it carries out often conflicting roles.

Many of the problems associated with highways in any town are a result of one or the other, or both of the basic flaws of today's roads and streets: (1) their inability to carry out all of their roles equally well, and (2) their inability to carry out these roles and provide a type of service for which they were never designed or built.

The following transportation inventory presents information necessary to develop a management plan for Wells future transportation system. It begins with general information with a description of how Wells residents act as users of the transportation system (section two), followed by a summary of the characteristics of the road network, including how it is managed and how it is used (section three), a discussion about bridges, sidewalks, and bicycle routes (section four), a summary on Town parking (section five), a summary of other modes of transportation available to residents of Wells (section six).

Well's Transportation System Users

Data from this section comes from the American Community Survey, and the Census Transportation Planning Products. The American Community Survey is conducted every year by the Census Bureau to supplement data collected every ten years as part of the official Census count. Like the Census, The American Community Survey collects demographic information to help determine funding allocations for each state. Demographic data included in the American Community Survey is not included in the Census process. The American Community Survey invites approximately one household out of every 38 households to participate. Participants are allowed to fill out paper forms or on-line forms. The Census Transportation Planning Products takes the American Community Survey data and allows creating easy to use maps with the data.

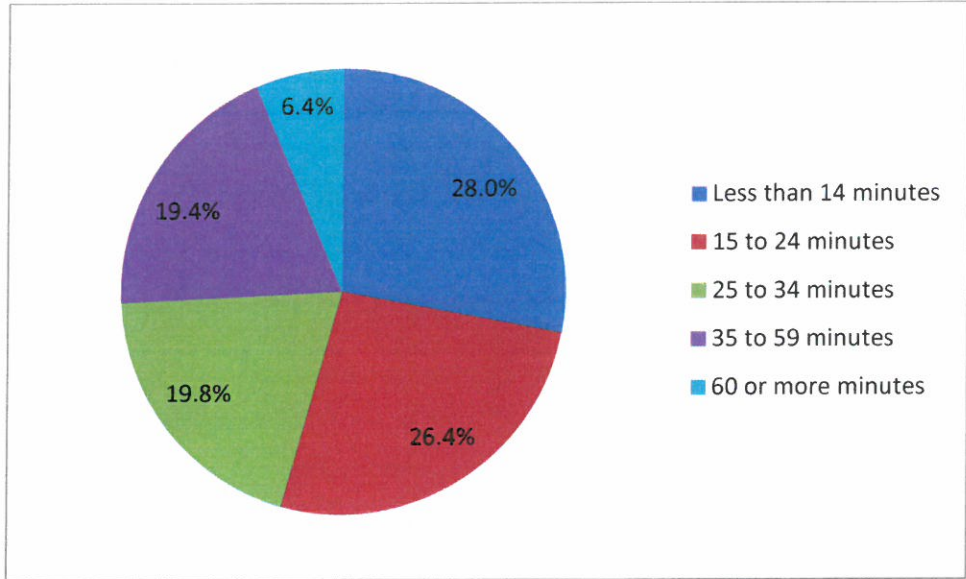
Like most Maine communities, the automobile supersedes all other modes as the predominant mode of transportation for Wells workers (nearly 76 percent of all workers). About 7 percent of all workers carpooled. While the automobile is dominant because of its convenience, it is notable that since there are alternatives in Wells, more people are choosing to use the options (see alternative transportation section).

Commuting to Work in Wells		
	Total	Percent
Car, Truck, or Van (Drive Alone)	3,092	75.8%
Car, Truck, or Van (Carpooled)	362	7.4%
Public Transportation	68	1.4%
Walked	137	2.8%
Bicycle	117	2.4%
Taxicab, motorcycle, or other means	15	0.3%
Worked at Home	293	6.0%
Total Commuters	4,895	100%
<i>Source: 2012 American Community Survey Data</i>		

Data indicates that most adults living in Wells have access to a vehicle, with approximately 2 percent of the population without access to a vehicle. More than half (53 percent) of all households have access to at least two vehicles and nearly 32 percent have access to at least three vehicles.

Vehicles Available in Wells		
	Total	Percent
0 Vehicle Available	117	2.4%
1 Vehicle Available	616	12.6%
2 Vehicles Available	2,596	53.1%
3 or More Vehicle Available	1,555	31.8%
Total Vehicles Available	4,889	100%
<i>Source: 2012 American Community Survey</i>		

A consistent traffic issue throughout Maine and the rest of the country is that traffic tends to build up during “commute hours,” or times when people go to and come back from work. Approximately 28 percent of people with jobs in Wells have less than a fourteen minute commute to work. Approximately 46 percent have commutes between 15 minutes and 34 minutes. 19 percent of all commuters travel between 35 and 59 minutes and the remainder of commuters (19 percent) has a commute greater than 60 minutes.



Most residents (nearly 72 percent) of Wells work in York County and only a small amount (nearly 15 percent) work outside of York County. Of those that work outside of York County, approximately 14 percent work outside of the state of Maine.

Place of Work	Percent
Worked in State of Residence	86.3%
Worked in County of Residence	(71.6%)
Worked Outside County of Residence	(14.7%)
Worked Outside State of Residence	13.7%
Total	100%

Source: 2012 American Community Survey

The Wells Road Network

The town’s roadway system ranges from rural country-type roads to the six-lane Maine Turnpike, which serves as the Maine’s gateway from the Boston metropolitan area and the eastern United States. The Maine Turnpike is a heavily traveled highway, especially during the ten-week summer tourist season from late June through Labor Day. The local road system experiences similar seasonal demands while the year-round population and travel activity in Southern Maine continues to rise.

Wells’s transportation network consists of 54.26 miles of public roadways. There are 0 miles of interstate, 9.03 miles of state highway roads, 0.28 miles of state aid highway roads, 40.90 miles

of town/seasonal roads, and 2.74 miles of additional seasonal roads. Road systems are grouped and classified for several reasons. Some important reasons to classify roads include:

- To design appropriate capacity, safety measures and design speed for roads.
- To guide investment priorities for roads.
- To provide a framework for a road maintenance program.
- To guide land use related regulations and access management standards with frontage on the roadway system.

Road Maintenance

There are four different jurisdictional categories used to classify how roads are maintained: state highways, state-aid highways, local roads, or private roads. The fourth category, which is important to distinguish is private roads and indicate that they are roads that are neither maintained nor owned by the town or the state.

State Roads are a system of connected main highways throughout the state which primarily serve arterial or through traffic. State highways are primarily maintained by the MaineDOT. The exceptions are the state roads located in urban compact areas, which are then maintained by the municipality. State Roads include Route 1, Route 9, Portland Road, and Route 109.

In 2013, MaineDOT performed several maintenance tasks in Wells related to the state roads outside of the urban compact area. These accomplishments are listed in the table below.

MaineDOT Activity Completed (2012)	
Drainage Structures Cleaned	1
Shoulder Miles of Sweeping	2.00
Linear Feet of Bridge Rail Repaired or Replaced	124.00
Bridges Cleaned	7
Bridges Washed	9
Square Feet of Bridge Piers Repairs	200
Bridge Inspections Performed	1
Ton(s) of Cold Patch Applied	6.50
Shoulder Miles of Mowing	42.00
Miles of Striping Applied	26.3 Miles
Linear Feet of Shoulder Rebuilt	150.00
Linear Feet of Guardrail or Fence Maintained	45.00
Shoulder Miles of Litter and Debris Removal	58.00
Linear Feet of Brush Removed	2,050
Minor Sign(s) Installed or Maintained	52
Miles of Shoulders Graded	0.50

Linear Feet of Brush Removed	1,100.00
<i>Source: MaineDOT</i>	

State aid highways are those highways not included in the system of the state highways which primarily serve as collector and feeder routes connecting local service roads to the arterial state highway system. Generally, State-Aid Highways in the rural area are maintained by the MaineDOT in the summer and by the town in the winter. Any State-Aid Highways in the urban compact area are maintained by the town. The State-Aid Highways include Route 9A and Route 9B.

Urban compact areas have populations greater than 7,000 and have sections of highway where buildings are closer than 200 feet for at least 0.25 miles. Under the rules of the Urban Compact Areas: the town of Wells and MaineDOT are required to complete the following maintenance:

Town of Wells	MaineDOT
Winter Snow and Ice Control	Route and Designation Signs
Pothole Repair	Bridge and Minor Span Maintenance
Pavement Markings per MUTCD (Centerline, Arrows, Words, Symbols, Crosswalks, and Edge Lines)	Speed Limit Signs on State or State Aid Highways when first installed or when changed due to MaineDOT Review
Traffic Signs per MUTCD (Regulatory, Warning, and Advisory)	
Ditching	
Driveway and Cross Culvert Cleaning, Repair, and Replacement	
Catch Basin Cleaning and Repair	
Surface Treatments i.e. Sand Seals, Chip Seals, Crack Sealing, Asphalt Shimming, Thin Overlays (typically less than 1 inch thick and do not improve strength)	
Traffic Signal Maintenance	
Guardrail Installation or Repair	
Railroad Signs and Pavement Markings	
Brush Cutting, Erosion Control, Mowing, Herbicide Application	
Tree Pruning or Removal	
Retaining Walls	
Sidewalk Maintenance	
Dust Control, Sweet Sweeping	
<i>Source: MaineDOT</i>	

Local roads are highways not included in the state highways and state-aid highway systems, which are maintained by the towns and primarily serve as local service roads providing access to

adjacent land. The town of Wells is also responsible for maintenance of local roads, which include local roads and roads that are included in the urban compact area.

Functional Classification

As was mentioned in the introduction to this chapter, many of the problems associated with highways is the fact that the highways' roles often conflict with one another, and all highways do not perform all of these functions equally well. Also, highways of today are often expected to perform functions and carry the type and amount of traffic for which they were never designed or built.

It is important, therefore, to understand the function of the highways in town in order to prevent their misuse and possible safety problems. From a standpoint of function, roads and streets can be classified into three (or more) functional classifications. For our purposes we have classified the roads in Wells as local roads and streets, collectors, or arterials.

Arterials – MaineDOT defines arterials as roadways that provide long-distance connections between towns and regional centers. Volumes of traffic typically range from 5,000 to 30,000 vehicles per day. Arterials are divided between *principle* arterials and *minor* arterials. MaineDOT, in its regulating driveways and entrances on state and state-aid highways, further classifies arterials into *mobility* and *retrograde* arterials. A *mobility* arterial corridor is a rural arterial with a posted speed limit of 40 miles per hour or more that carries 5,000 or more vehicles per day for at least fifty percent of its length. A *retrograde* arterial corridor is a type of *mobility* arterial on which crash rates due to vehicles entering and existing driveways exceed the 2001 state average crash rate of such crashes. Arterials roads in Wells include:

- *Principle Arterials*: 1-95/Maine Turnpike
- *Other Freeways and Expressways*: None
- *Other Principle Arterials*: Route 109
- *Minor Arterials*: US 1, Route 4, Route 9

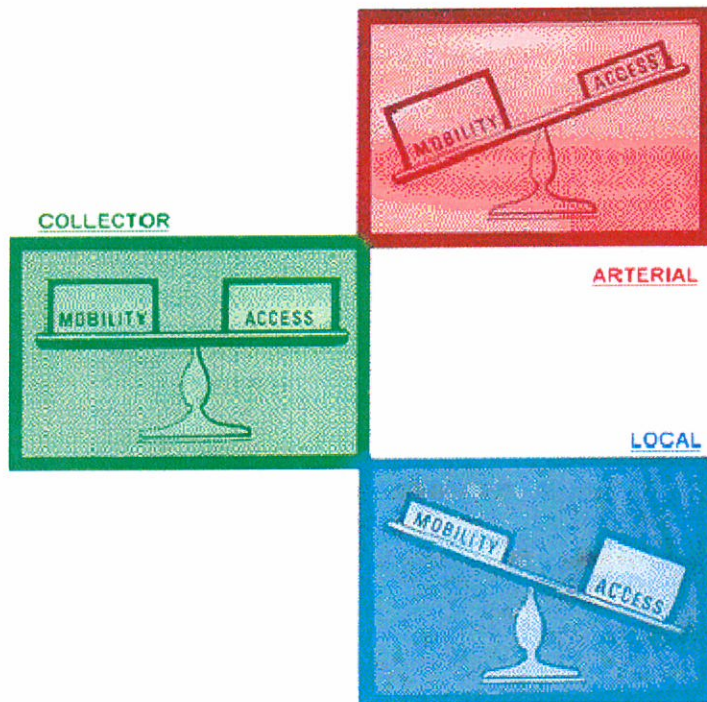
It is here at the arterial level where the conflict between the traffic service function of the highway and the land or property service function is most extreme. As traffic volumes increase and land use intensifies, this conflict also increases. There are three ways to eliminate or prevent this conflict from causing possible safety problems associated with misuse: (1) provide additional capacity in the highway (additional lanes), (2) provide additional highways, or (3) manage the access to the existing arterial highways.

Collectors – Collectors act as connecting roads between local or residential neighborhoods and arterials. Collectors also provide an element of community aesthetics. These roadways are the locations from which many of us view our community. Traffic is *collected* from local roads and delivered to arterial roadways, which are designed for higher speed and improved mobility. Typically, traffic volumes on collector roads range from 1,000 to 5,000 vehicles per day. Like

arterials, MaineDOT further divides classification of collectors into *major* and *minor* collectors. MaineDOT requires driveway and entrance permits for all collector roads. Collector roads in Wells include:

- *Major Collectors:* Route 9A from Route 109 to the Kennebunk Town Line and Route 9B from Route 1 to Route 1
- *Minor Collectors:* Bragdon Road

Local Roads – Local roads and streets provide access to individual parcels of land. Moving traffic is only of secondary importance. Volumes typically carry up to 1,000 vehicles per day. All roads not classified by MaineDOT as arterial or collectors are considered local roads. Local roads maybe be town-owned or private and its important for towns to make that distinction. Local roads and streets are also a part of the residential (and sometimes commercial and industrial) design features. The curves, straight sections, street trees, landscaping, street lighting, and so forth can provide a strong element of community aesthetics. Local roads and streets also provide the sites for building.



Source: *MaineDOT Website*

Local Roads Assistance Program

The MaineDOT has a system to help municipalities maintain local and minor collector roads. In 1999, MaineDOT adopted the Urban Rural Initiative Program (URIP). Beginning July 1, 2013 URIP became known as the Local Road Assistance Program (LRAP). The new program

eliminates the *hold harmless* portion of the law. The LRAP continues to be focused on municipal aid toward highway and bridge *capital improvements*. Prior to 1999, the use of these *local road* funds was only for the *maintenance or improvement of public roads*. Since 1999, these funds must be used for *capital improvements* to local roads. The table below indicates the LRAP funding the town of Wells has received and will receive during federal fiscal year 2014. All towns are receiving less funding during Fiscal Year 2014 because the total LRAP funding was reduced for Fiscal Year 2014 and 2015.

Fiscal Year	Total Funding
2015	\$200,664
2014	\$237,404
2013	\$230,810
2012	\$240,991
2011	\$232,391
<i>Source: MaineDOT Local Roads Program</i>	

Capital Improvements

There are two different entities that fund the road system in Wells: the town and MaineDOT. The town of Wells spends town funding on maintaining and improving local roads. There are several different sources of funding from MaineDOT that are available to Wells. Beginning 2013, MaineDOT will be listing projects in a calendar year method instead of federal fiscal year. The federal fiscal year begins in October and ends in September.

MaineDOT Work Plan – The MaineDOT’s new Calendar Year 2014-2015-2016 Work Plan (Work Plan) supports the department's mission, "To responsibly provide our customers with the safest, most reliable transportation system possible, given available resources." This Work Plan contains projections of transportation resources (federal, state, other) and MaineDOT’s strategy to apply them to the planning, engineering, construction, operation and maintenance of transportation infrastructure of all modes throughout Maine. The Work Plan emphasizes focusing scarce transportation resources on existing critical infrastructure needs—primarily roads and bridges—to the greatest extent possible. Projects that are included in the MaineDOT Work Plan focus on airports, bridges, and road infrastructure.

Project ID: 020477.00

Calendar Year: 2014

Scope of Work: Bridge Construction

Highway Corridor Priority: 1

Location: Wells, Ogunquit

Description: Donnell's Bridge (#2239) which carries Route 1 over the Ogunquit River at the Wells/Ogunquit Town Line.

Estimated Funding: \$1,500,000

Project ID: 020203.00

Calendar Year: 2014

Scope of Work: Highway Safety and Spot Improvements

Highway Corridor Priority: 1

Location: Wells

Description: Located at the intersection of Route 1 and Route 109.

Estimated Funding: \$195,000

Project ID: 020566.00

Calendar Year: 2014

Scope of Work: Highway Safety and Spot Improvements

Highway Corridor Priority: 1

Location: Wells

Description: Interconnect and modify traffic signals and upgrade pedestrian facilities at Mile Road and at Route 9B.

Estimated Funding: \$1,958,500

Project ID: 020474.00

Calendar Year: 2015-2016

Scope of Work: Bridge Construction

Highway Corridor Priority: 1

Location: Wells, Kennebunk

Description: Pumping Station Bridge (#2693) which carries Route 1 over Branch Brook at the Wells/Kennebunk Town Line.

Estimated Funding: \$800,000

Project ID: 020278.00

Calendar Year: 2015-2016

Scope of Work: Highway Preservation Paving

Highway Corridor Priority: 1

Location: Wells

Description: Beginning 0.13 of a mile north of the Ogunquit/York Town Line extending northerly 3.72 miles to 0.06 of a mile north of South Street.

Estimated Funding: 855,600

Project ID: 022673.00

Calendar Year: 2015-2016

Scope of Work: Highway Safety and Spot Improvements

Highway Corridor Priority: 1

Location: Wells

Description: Reconstruct Chapel Road approach to 90 degrees.

Estimated Funding: \$165,000

Project ID: 022704.00

Calendar Year: 2015-2016

Scope of Work: Highway Safety and Spot Improvements

Highway Corridor Priority: N/A

Location: Wells

Description: Replace flashing beacon at Laudholm Farm Road, at Drake's Island Road in Kennebunkport, and at Durrell's Ridge Road in Wells.

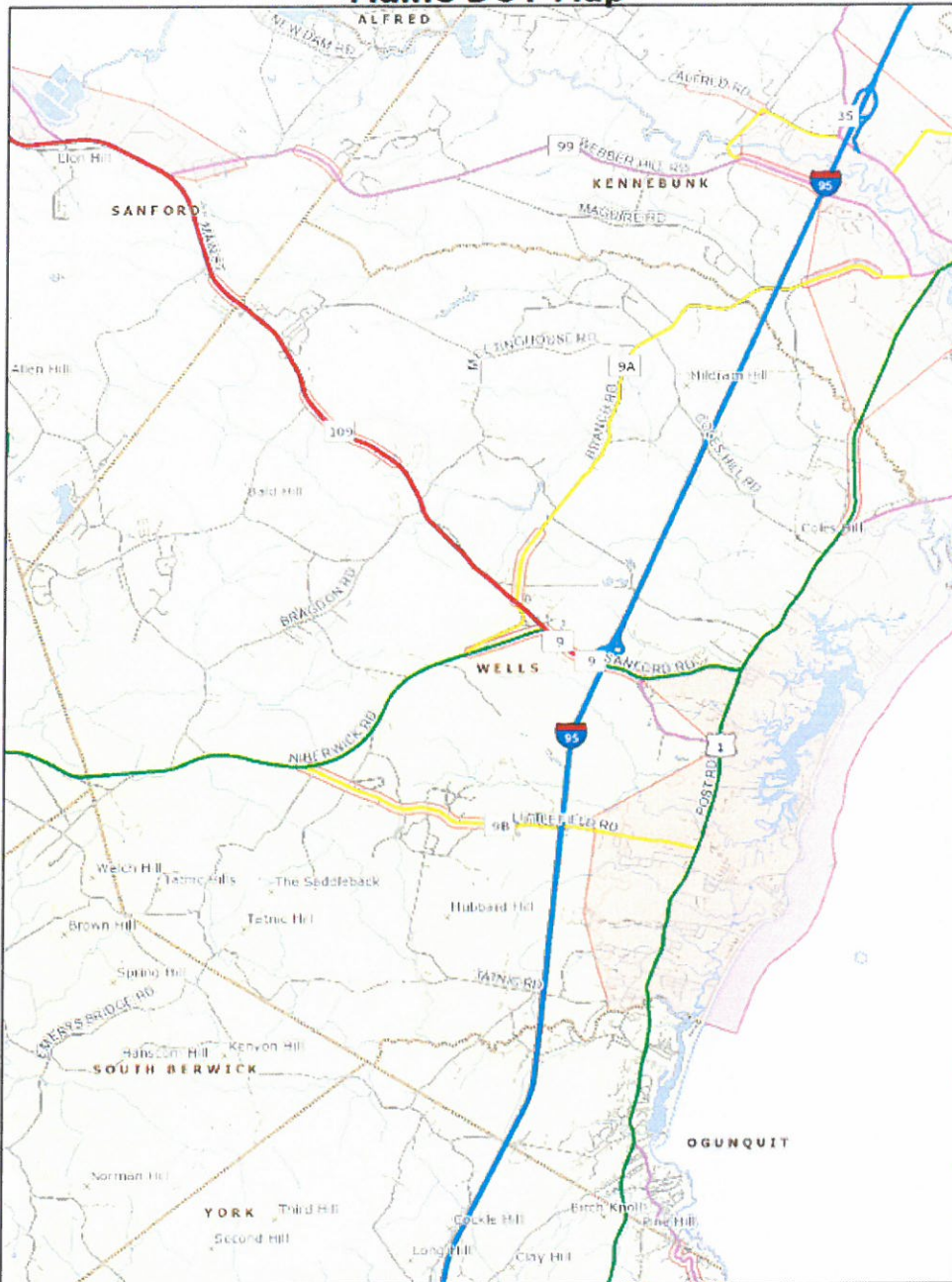
Estimated Funding: \$80,000

Municipal Partnership Initiative (MPI) – The MPI is a new program to MaineDOT that began in 2011. The program is geared towards funding projects for state and state-aid highways that often get overlooked. Municipalities that would like to participate are required to contribute at least half of the project costs. The remainder of the project will be funding with state funds. It is expected that the municipality and not MaineDOT will manage the project. MaineDOT will reimburse entities once the work is complete to the satisfaction of MaineDOT. All MPI projects must be certified by a professional engineer and have a useful life span of at least ten years. Municipalities may propose shifting long-term maintenance responsibilities as part of their share.

Asset Management

MaineDOT Map Viewer – The MaineDOT Map Viewer is an online mapping program designed for municipalities to collect a variety of transportation data. There are many functions, but the most useful functions include mapping of federal functional road classifications, bridge and railroad data, MaineDOT transportation projects, and Highway Corridor Priorities and Customer Service Levels. The Map Viewer can be found on the MaineDOT website. For more information, visit: <http://www.maine.gov/mdot/mapviewer>.

Maine DOT Map



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MaineDOT Highway Corridor Priorities – The MaineDOT Highway Corridor Priorities are based on a ranking system. The following chart outlines the priority system for the current roadway system. The Corridor Priorities are based on federal functional classification system, regional economic significance, heavy haul truck use and relative regional traffic volumes.

- Priority 1 – Route 1, Route 4, , Route 109, and the Maine Turnpike
- Priority 2 – Route 9/North Berwick Road

- Priority 3 – None
- Priority 4 – Route 9/Portland Road
- Priority 5 – Route 9A, Route 9B

Priority	Definition
Priority 1 Roads	These roads include the Maine Turnpike, the interstate system and key principal arterials like Route 1 in Aroostook County, the Airline (Route 9), Route 2 west of Newport, and Route 302. The 1,400 miles of Priority 1 roads represent only 7 percent of the miles, but carry fully 40 percent of all vehicle miles traveled in Maine.
Priority 2 Roads	These roads total about 940 miles. They are non-interstate, high value arterials that represent about 4 percent of the total miles of road but carry 11 percent of overall traffic.
Priority 3 Roads	These roads generally are the remaining arterials and most significant major collector highways. These 2,050 miles represent only 9 percent of miles, but carry 19 percent of the traffic.
Priority 4 Roads	These roads generally are the remainder of the major collector highways, often also part of Maine's unique state aid system, in which road responsibilities are shared between the state and municipalities. These 1,900 miles represent about 8 percent of total miles, and carry 10 percent of the traffic.
Priority 5 Roads	These roads are 2,500 miles of minor collector highways, almost all on the state aid system. They represent 11 percent of miles, but carry only 7 percent of traffic.
Priority 6 Roads	These roads are local roads and streets, and are the year-round responsibility of our municipal partners. Though they carry just 13 percent of the statewide traffic, these 14,300 miles make up 61 percent of the total miles.

MaineDOT Customer Service Levels – Like the Highway Corridor Priorities, the Custer Service Level is prioritized on three criteria: safety, condition, and service. Each criterion has several factors that are included in the overall rating of each category. Roads and road segments are

given an A-F rating with A being the best and F being the worst. To get a better idea of the customer service levels for each road/node, visit the MaineDOT map viewer program.

- Safety – Most roads in Wells are classified in the B or C category. There is a small section of Route 9A near Route 109 that is classified as D due to crash history and pavement width and two small sections of Route 109 classified as an F due to crash history and pavement width.
- Condition – The majority of roads in Wells are classified in the A or B Category. There are no sections of road that classified as a D or F.
- Service – Almost all roads in Wells are classified in the A or B category. There are segments of Route 1 and Route 9 classified as a D and F due to congestion.

Customer Service Level	Category	Definition
Crash History	Safety	This measure includes the two types of motor vehicle crashes most likely related to the highway- head-on and run-off road crashes. The A-F scale compares these crash rates with the statewide average.
Paved Roadway Width	Safety	This measure compares total paved width (lane plus shoulder) with minimum acceptable widths by Highway Corridor Priority (not new design standards). If a highway segment fails this minimum, the Safety Customer Service Levels for that segment is decreased one letter grade.
Pavement Rutting	Safety	This measure looks at wheel path rutting, since excessive rutting holds water and contributes to hydroplaning and icing in winter. The A-F scale set points vary by Highway Corridor Priority, and are based on hydroplane tests.
Bridge Reliability	Safety	This measure is pass/fail. If a highway segment contains a bridge with a Condition Rating of 3 or less (excluding non-overpass decks), the Safety Customer Service Level is decreased one letter grade. These bridges are safe, but may require increased inspection or remedial work that could affect traffic flow.
Pavement Condition	Condition	This measure uses the Pavement Condition Rating (PCR), a 0-5 scale that is composed of International Roughness Index, rutting, and two basic types of cracking. The A-F scale varies by Highway Corridor

Customer Service Level	Category	Definition
		Priority.
Roadway Strength	Condition	This measure uses the results of the falling weight deflectometer, a device that estimates roadway strength. The A-F scale is uniform across Highway Corridor Priority, since even low-priority roads must support heavy loads in Maine's natural resource-based economy.
Bridge Condition	Condition	This measure converts the 0-9 national bridge inventory (NBI) condition ratings to pass or fail; it is uniform across Highway Corridor Priority.
Ride Quality	Condition	This measure uses the International Roughness Index (IRI), which is expressed in inches per mile of deviation. IRI is the nationally accepted standard for passenger comfort, and the A-F scale varies by Highway Corridor Priority.
Posted Road	Service	Each year, MaineDOT posts more than 2,000 miles of road during spring thaw to protect their longevity, but some posted roads directly affect Maine's economy. Road segments that are permanently posted get a D, those with seasonal postings get a C.
Posted Bridge	Service	This measure uses load weight restrictions to arrive at an A-F score that varies by Highway Corridor Priority.
Congestion	Service	This measure uses the ratio of peak traffic flows to highway capacity to arrive at an A-F score for travel delay. Peak summer months are specifically considered to capture impacts to Maine's tourism industry. This scale is uniform across Highway Corridor Priority, since tourist travel is system-wide and sitting in traffic affects customer service similarly on all roads.

Traffic Volumes – The Average Annual Daily Traffic (AADT) is the predominant type of traffic data that is collected for Maine roadways. In some ways, traffic volume trends are an excellent

way to collect the functionality of the road system. MaineDOT is responsible for conducting traffic counts for the Southern Maine Planning & Development Commission. Wells is part of the Zone 1 and traffic counts are conducted every 3 years.

Traffic Counts: 2007, 2010, 2013				2007-2013		2010-2013	
	2007	2010	2013	Change	Percent	Change	Percent
US 1/SR 9 NE/O Harbor Road	--	12,460	12,960	--	--	500	3.86%
US 1 N/O Mile Road	--	20,120	17,070	--	--	-3,050	-17.87%
US 1 S/O Mile Road	--	17,970	15,360	--	--	-2,610	-16.99%
US 1 S/O Tatnic Road at Bridge #2239 and Town Line	14910	--	13,370	-1,540	-12%	--	--
SR 9A (Branch Road) SW/O Meeting House Road	1920	1,920	2,140	220	10%	220	10.28%
SR 9B (Littlefield Road) SE/O SR 9 (North Berwick Road)	2600	2,540	3,090	490	16%	550	17.80%
SR 9/109 (Sanford Road) E/O Chapel Road	--	--	9,420	--	--	--	--
SR 9/109 (Sanford Road) W/O Chapel Road	--	--	14,970	--	--	--	--
SR 9 (North Berwick Road) E/O SR 9B (Littlefield Road)	6120	6,080	6,360	240	4%	280	4.40%
SR 9 (North Berwick Road) W/O SR 9B (Littlefield Road)	7980	7,800	8,460	480	6%	660	7.80%
SR 9 (North Berwick Road) SW/O SR 9/109 (Sanford Road)	6880	6,980	6,980	100	1%	0	0.00%
Quarry Road SW/O Bald Hill Road	740	890	820	80	10%	-70	-8.54%
SR 109 (Sanford Road) SE/O SR 9A (Branch Road)	9240	9,180	8,800	-440	-5%	-380	-4.32%
SR 109 (Sanford Road) NW/O SR 9A (Branch Road)	8190	7,770	8,490	300	4%	720	8.48%
SR 109 (Sanford Road) SE/O Wire Road	7140	7,930	7,630	490	6%	-300	-3.93%

Road Safety – The table below indicates that there were 1,666 crashes in Wells between 2008 and 2013. From 2008 to 2013, there has been a decrease in crashes by 61 (24.80 percent). 2008 had the most crashes with 307. 2010 had the least amount of crashes with 254.

Town	2008	2009	2010	2011	2012	2013	TOTAL
Wells	307	301	254	290	268	246	1,666

MaineDOT has a system that it uses to rate crash locations through out the state called the *High Crash Locations*. High Crash Locations are given greater attention for funding projects by MaineDOT for their safety programs. In order to qualify, High Crash Locations must be at locations that have had at least eight crashes in the same location for a three-year period. It also must exceed the Critical Rate Factor of crashes. A Critical Rate Factor is the average expected rate of crashes for a location (based on statewide data of similar crashes). In Wells, there were six high crash locations between 2011 and 2013. The number one high crash location in York County was located at the intersection of Chapel Road and US 1 with 38 accidents. This intersection will be improved in 2015-2016 by the MaineDOT. Chapel Hill at then intersection of US 1 will be reconstructed to a 90 degree approach.

	Total Accidents	Critical Rate Factor	Ranking County/State
High Crash Locations in Wells: 2011-2013			
Intersection of Chapel Road and US 1	38	5.76	1/19
Intersection of Garden Street, North Berwick Road, and Sanford Road	12	2.24	22/136
Intersection of Chapel Road and Sanford Road	10	1.68	33/178
Intersection of Branch Road, Crediford Road, and Sanford Road	16	3.99	6/50
US 1 between Mile Road and Bayview Terrace	21	1.04	32/164
Intersection between US 1 and Mile Road	13	1.15	24/153

Access Management: For improved safety and speed preservation along the state's highways, the MaineDOT has developed a set of access management rules in response to legislation concerned with arterial capacity, poor drainage, and the high number of driveway-related crashes. Any new or changed driveway or entrance on state and state aid highways located outside of urban compact areas must meet specifications described in the rules in order to obtain a permit from MaineDOT. The rules regulate sight distance, corner clearance, spacing, width, setbacks, parking, drainage, and mitigation requirements.

The rules are organized into a four-tier system with regulation of driveways and entrances increasing for roads with higher mobility importance and poorer safety records. The following are the designations for the highway networks in Wells:

The rules define mobility corridors as those corridors that connect service centers and/or urban compact areas and carry at least 5,000 vehicles per day along at least 50% of the corridor's length. In Wells, the mobility corridors include the non-urban compact portions of the following roads:

- Basic safety standards apply to all state and state-aid roads. In Wells, this includes the non-urban compact portions of Route 9, Route 9A, Route 9B, Route 109, and US 1.
- Major collector and Arterial standards provide more detailed design standards for entrances into major collector and arterial roads. Entrances are access that serves 50 or more trips per day. In Wells, this includes the non-compact portions of Route 9, Route 109, and US 1.
- Mobility corridors connect service centers and/or urban compact areas and carry at least 5,000 vehicles per day along at least 50 percent of the corridors length. In Wells, this includes the non-urban compact portions of Route 9 west of Route 109, Route 109 north of Route 9, and US 1.
- Retrograde arterials are mobility corridors where the number of crashes related to a driveway or entrance exceeds the statewide average for arterials with the same posted speed. In Wells, this includes the non-urban compact portions of Route 9 between Boyd Road and Route 109, Route 109 between Route 9 and El Velvel Circle, and a portion of US 1 just south of the Kennebunk town line.

Corridor/Transportation Studies

Wells Town Center Development Plan – In 2002, the town of Wells created an Advisory Committee to create the Central Area Development Plan. The goal of the plan was to look at ways to create a Town Center centered in the Wells Corner area and extending north along Route 1 and west along Route 109. Included in this area are the Town Hall, schools, and other local businesses that would help shape the direction of a new Town Center.

Route 109 Corridor Study – In 2004, the towns of Wells and Sanford completed a Route 109 Corridor Study with assistance from the Southern Maine Planning and Development Commission. The plan primarily focused on improving access management along the Route 109 Corridor in Wells and Sanford. The Route 109 Corridor Committee assisted the MaineDOT in designing the reconstruction of Route 109. That project was completed in 2013 and improved safety of Route 109 by widening the shoulders and improving horizontal and vertical alignments and sight distance.

HUD Center of Opportunity – In 2013, the town of Wells participated in the Sustain Southern Maine project as a Center of Opportunity. The project focused on the region of Wells near the Transportation Center to Route 1 and then Route 1 from the intersection of Mile Road to the intersection with Route 109. There are many discussions and recommendations from the Center

of Opportunity project that can be found on the Sustain Southern Maine website (www.sustainsouthernmaine.org).

Central York County Connections Study – Wells was a participant in the Central York County Connections Study. The study was completed through the Maine Department of Transportation and Maine Turnpike Authority. The purpose of the study was to identify, evaluate and recommend feasible transportation options for the Central York County region. More information can be found in the report.

1. Bridges, Sidewalks, and Bicycle Routes

Bridges: There are 43 bridges in the town of Wells, with 18 owned/maintained by MaineDOT, 10 owned/maintained by the town of Wells, 2 owned/maintained jointly by Wells and Ogunquit, 12 owned/maintained by the Maine Turnpike Authority, and 1 owned/maintained privately by the railroad. Information on these bridges is provided in the table below.

Bridge condition is monitored every two years and given a Federal Sufficiency Rating (FSR). Each FSR has a numeric indicator of the overall value of the sufficiency of the bridge. A rating will be from 0-100 (0 indicates the worse and 100 indicates the best). FSR is computed with a federally supplied formula using an array of condition and inventory data. The formula is used to identify bridges eligible for federal funding. The FSR includes both structural deficiencies as well as functional obsolescence. This rating gives an overall value of the sufficiency of the bridge. Since functional obsolescence (too narrow or low weight capacity) may account for a large portion of the rating, one should not assume that a low sufficiency rating means the bridge could fail.

Bridge Name	Bridge Number	Owner/Maintenance	Sufficiency Rating
Dickens Hill	1252	MaineDOT	81.8
Donnell's	2239	MaineDOT	75.7
Tibbetts	1254	Wells/Ogunquit	23.3
Ogunquit River Bridge	1317	Wells/Ogunquit	77.1
Webhannet River	1320	Wells	77.5
B&M Railroad Overpass SB	1478	Wells	92.6
Branch Brook	1328	Maine Turnpike Authority	77.4
Cols Hill Road	1327	Maine Turnpike Authority	93.5
Merriland River Bridge	1326	Maine Turnpike Authority	76.8
Burnt Mill Road	1325	Maine Turnpike Authority	97.9
Wells Interchange NB	1324	Maine Turnpike Authority	94.3
Wells Sanford Road NB	1323	Maine Turnpike Authority	99.4

US Route 1 Over B&M Railroad	1102	MaineDOT	87.5
Webhannet Branch	1321	Maine Turnpike Authority	78.0
Buffam	2107	MaineDOT	65.0
Littlefield Road	1319	Maine Turnpike Authority	92.3
Tatnic Road	1318	Maine Turnpike Authority	95.7
Jeffred Branch	1267	MaineDOT	99.8
Camp	1259	Wells	88.8
Clarks	1255	Wells	85.9
Stover Mill	1253	Wells	79.9
B&M Railroad Overpass NB	1322	Maine Turnpike Authority	81.4
Bragdon Crossing	3200	MaineDOT	89.9
Hilton	6216	Wells	38.5
Sherburne	6122	MaineDOT	99.0
Merriland Ridge Bridge	5338	MaineDOT	55.6
B&M Railroad Overpass	5337	Railroad	-1.0
Lewis West	3916	Wells	48.1
Charles West	3915	Wells	73.3
Bert Wells	3844	MaineDOT	79.0
Skinner	3771	MaineDOT	84.0
Wells Sanford Road SB	1479	Maine Turnpike Authority	96.4
Drakes Island	3577	Wells	47.8
Wells Interchange SB	1480	Maine Turnpike Authority	94.5
High Pine Crossing	3199	MaineDOT	85.1
Island Ledge Road	3175	MaineDOT	80.1
Branch Brook	3091	MaineDOT	79.1
Pumping Station	2693	MaineDOT	64.9
Little River	2468	MaineDOT	85.8
Edward Hill	2263	MaineDOT	72.2
Capell	2126	MaineDOT	74.0
Merriland River	6492	Wells	86.9
Bourne Avenue	3765	MaineDOT	80.8

2. Pedestrian & Bicycle Infrastructure

The town of Wells has access to several options for pedestrian and bicycle infrastructure to encourage residents and visitors to walk and bike.

Trails: There are two significant organizations that have created on-road and off-road trails that travel through Wells. The Eastern Trail Alliance created the Eastern Trail network that connects Kittery to South Portland through a series of on-road and off-road trails. In Wells, the Eastern Trail is located on US 1. The Eastern Trail is part of the larger East Coast Greenway network

that will eventually connect Maine to Florida through a trail system. Another initiative is US Bike Route 1...

Pedestrian Network: Pedestrians are a part of every roadway environment, and attention should be paid to their presence. Sidewalks are the primary facility provided to meet their needs, and care must be taken when designing a pedestrian network to account for all users. This includes children, the elderly, parents with strollers, and pedestrians with physical disabilities and impairments that require the use of wheelchairs and other assistive devices. Future sidewalks will be included on Harbor Road and will be extended south along Route 1.

Bicycle Network: Bicyclists have the same mobility needs as any other road user. Increasingly land use and transportation planners are recognizing the bicycle as a viable transportation mode. While recreation is still the primary use of the bicycle, more people are beginning to cycle as a way to commute to work and run errands. Cyclists should be included in all phases of transportation planning including new road design, construction, and rehabilitation.

Any segment of roadway having a paved shoulder of at least four feet in width is generally considered appropriate for bicycle travel. A portion of Route 109/9 has a bicycle lane. Future bike lanes will be included on Harbor Road and Furbish Road. As highways are improved and upgraded, the town will encourage adequate shoulder widths in order to accommodate bicycle travel.

3. Parking

There are limited opportunities for parking in the town of Wells. There is limited parking for many of the more popular tourist attractions including Wells Beach, Moody Beach, Wells Harbor Park, Drake's Island Beach, the Wells Reserve/Laudholm Beach. Those locations and roads leading to them experience parking issues during the tourist months. There is parking available at businesses along US 1 and at the Wells Transportation Center, which provides parking for the Amtrak Downeaster and a park and ride lot for commuters. There is limited on-street parking on Ocean Avenue and Atlantic Avenue. There is need for additional off-street parking adjacent to US 1 that can be serviced by the trolley to shuttle visitors to the beaches.

4. Other Modes of Transportation

Wells' transportation system is in large part a reflection of the historical growth of the town. At the end of the 19th Century and beginning of the 20th Century, a light electrical rail service was available in Wells and other nearby coastal southern Maine Communities. The rail service connected the coastal southern Maine communities with Sanford and Springvale. Long distance railroads with two stations in Wells served the town. Similar to the pattern that evolved throughout the United States during the early to middle part of the 20th Century, the private automobile became the primary means of transportation in Wells.

While motorized vehicles including automobiles, heavy and light trucks, will continue to be the primary form of transportation in Wells, the Town has been effective in encouraging and planning for other forms of transportation. The most significant example of this planning is the addition of the Wells Transportation Center, which opened for business in December 2002. The Wells Transportation Center is located directly off of Exit 19 of the Maine Turnpike Authority on Route 109/9. It is an intermodal transportation facility serving Southern Maine by passenger rail service and commuter parking for 220 vehicles.

Public Transportation: The York County Communication Corporation (YCCAC) provides several forms of public transportation for the town of Wells.

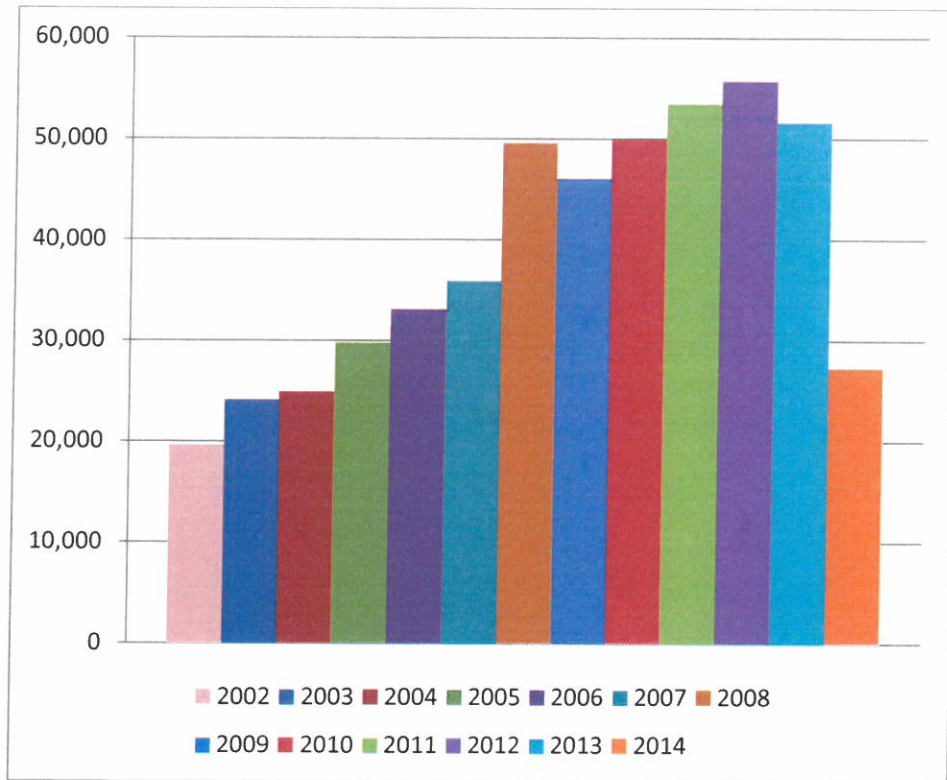
The WAVE transports riders to training and job sites, and their children to day care, with general public trips on a space-available basis. The blue and white vans and buses operate 7 days a week. Trips must be arranged in advance. The current service is for an area including Sanford - Springvale and parts of Wells and Wells, and parts of Lyman and Biddeford.

- The Shoreline Explorer provides seasonal trolley service to coastal communities in southern York County. There are four routes that provide seasonal transportation with two that provide services in the town of Wells (The Blue Line/Route 4 and the Orange Line/Route 5). The Blue Line/Route 4 serves Ogunquit, Wells, and Kennebunk. The Orange Line/Route 5 serves Wells and Sanford and provides full-year service.

Route	June		July		August		September	
	2013	2014	2013	2014	2013	2014	2013	2014
Blue Line	3,763	4,820	24,733	26,191	28,764	27,447	1,097	381
Orange Line	303	349	1,303	1,239	1,508	1,508	81	28
<i>TOTAL</i>	<i>4,066</i>	<i>5,169</i>	<i>26,036</i>	<i>27,430</i>	<i>30,272</i>	<i>28,995</i>	<i>1,178</i>	<i>409</i>

- The WAVE (Wheels to Access Vocation and Education) service provides riders with the opportunity to travel to jobs and to bring their children to daycare. If space is available, the service is open to the general public. The WAVE service is provided year round. In 2013, they YCCAC provided 34,182 one-way trips though the WAVE service.
- YCCAC also provides transportation services for medical/non-emergency and shopping purposes for communities in York County. They provide a weekly service for Wells that connects to the hospital and shopping locations in Biddeford.

Rail: Passenger Rail service is provided through the Amtrak Downeaster service at the Wells Transportation Center. With the exception of 2009 and 2013, ridership has increased each year since the service began in 2002. Note that 2014 only includes months from January to July 2014.

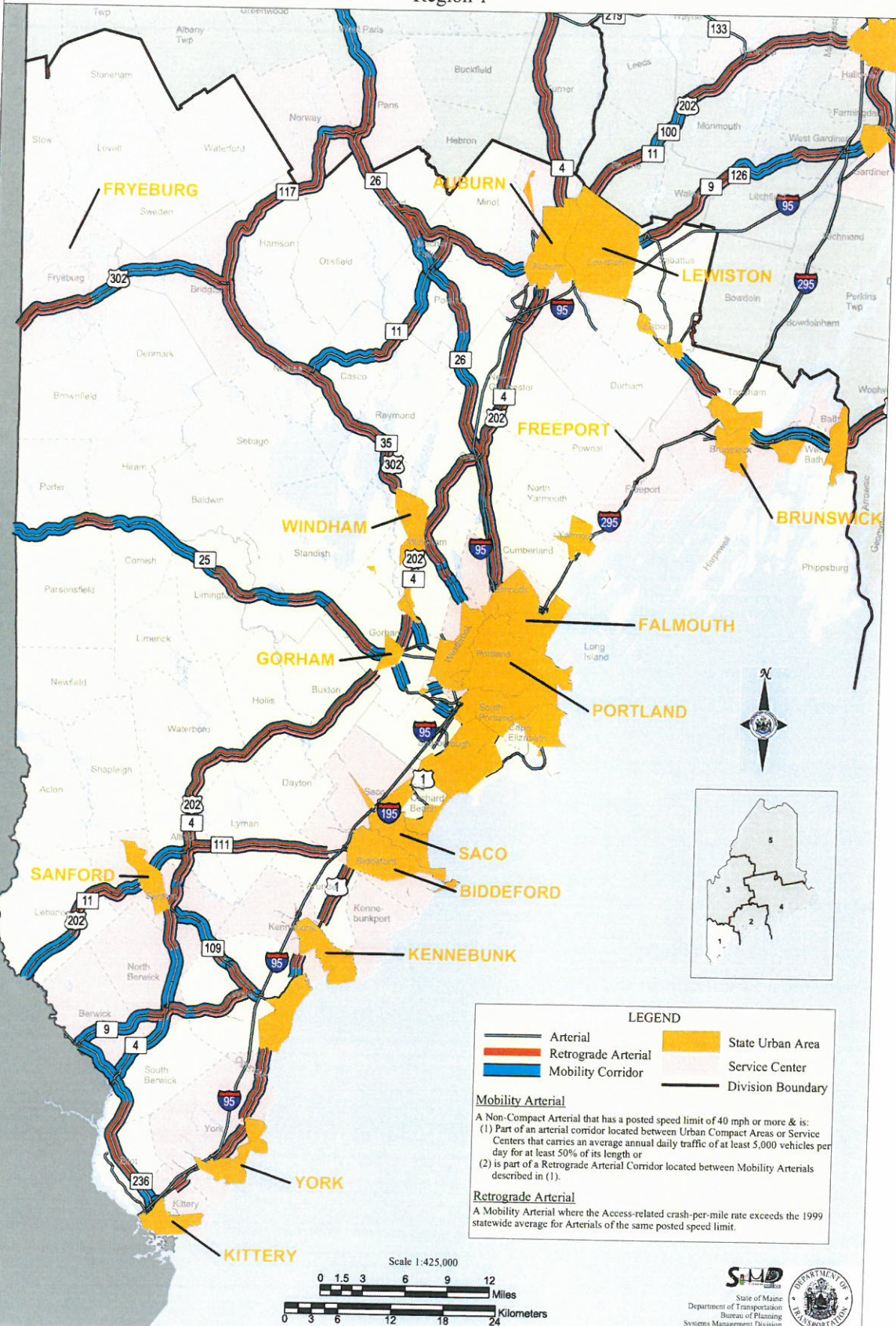


Air: Wells is approximately 30 minutes from the Portland International Jetport and the Pease International Tradeport in Portsmouth, New Hampshire. Airports in Manchester, New Hampshire or Boston are approximately 75 minutes from Wells. Shuttle service is available to Boston and Portland from private carriers.

MaineDOT Access Management Program

Mobility Corridors & Retrograde Arterials

Region 1



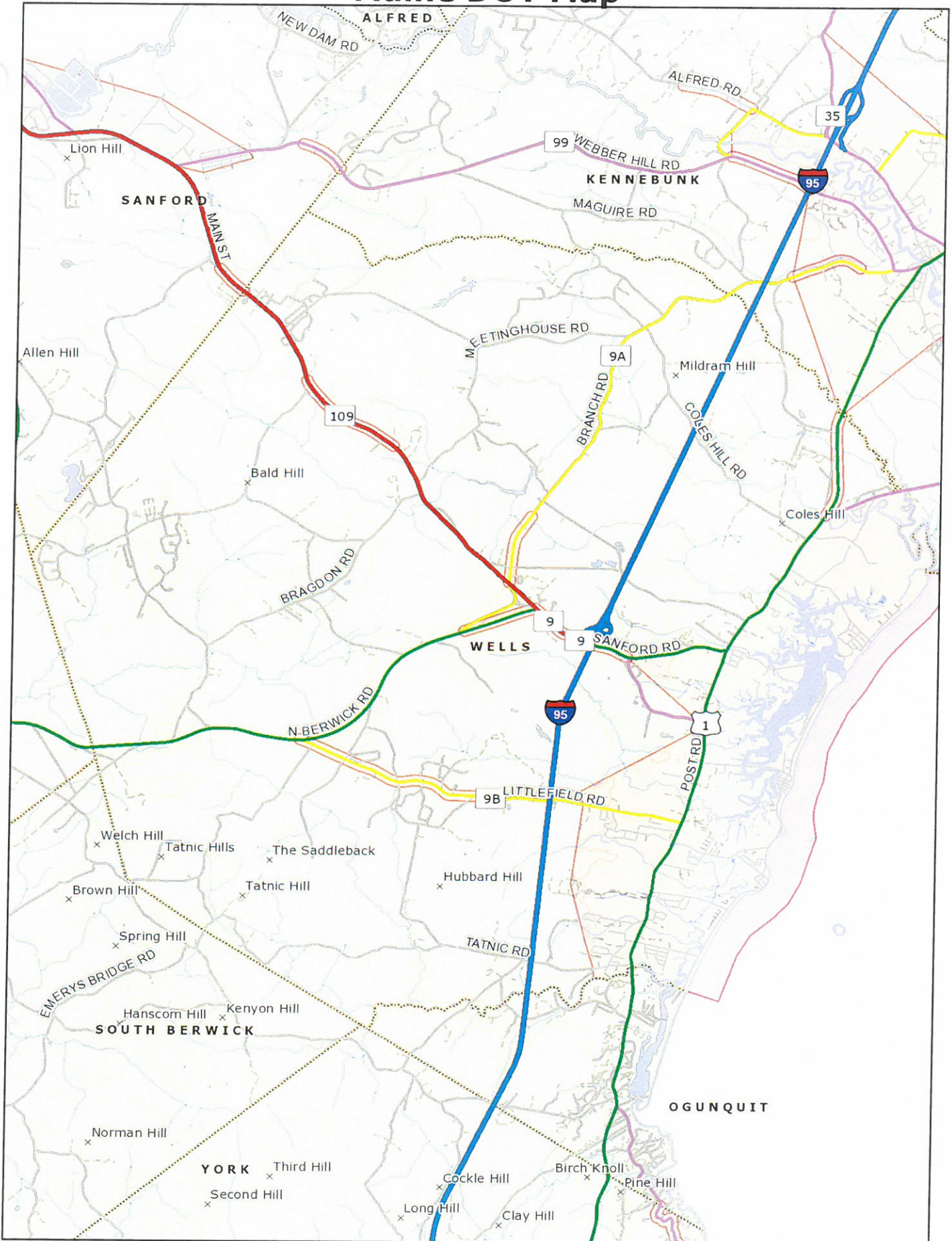
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State of Maine Department of Transportation Bureau of Planning Systems Management Division Transportation Reporting, Inventory & Mapping

Bridge Name	Bridge Number	Owner/Maintenance	Sufficiency Rating	Last Inspection
Dickens Hill	1252	MaineDOT	81.8	10/30/1995
Donnell's	2239	MaineDOT	75.7	06/01/2011
Tibbetts	1254	Wells/Ogunquit	23.3	10/30/1995
Ogunquit River Bridge	1317	Wells/Ogunquit	77.1	05/12/2012
Webhannet River	1320	Wells	77.5	05/02/2012
B&M Railroad Overpass SB	1478	Wells	92.6	05/11/2012
Branch Brook	1328	Maine Turnpike Authority	77.4	05/02/2012
Cols Hill Road	1327	Maine Turnpike Authority	93.5	05/09/2012
Merriland River Bridge	1326	Maine Turnpike Authority	76.8	05/09/2012
Burnt Mill Road	1325	Maine Turnpike Authority	97.9	02/20/2013
Wells Interchange NB	1324	Maine Turnpike Authority	94.3	05/09/2012
Wells Sanford Road NB	1323	Maine Turnpike Authority	99.4	05/09/2012
US Route 1 Over B&M Railroad	1102	MaineDOT	87.5	11/01/2012
Webhannet Branch	1321	Maine Turnpike Authority	78.0	05/02/2012
Buffam	2107	MaineDOT	65.0	05/21/2012
Littlefield Road	1319	Maine Turnpike Authority	92.3	05/11/2012
Tatnic Road	1318	Maine Turnpike Authority	95.7	05/11/2012
Jeffred Branch	1267	MaineDOT	99.8	04/04/2013
Camp	1259	Wells	88.8	01/21/2014
Clarks	1255	Wells	85.9	01/30/2014
Stover Mill	1253	Wells	79.9	01/22/2014
B&M Railroad Overpass NB	1322	Maine Turnpike Authority	81.4	05/11/2012
Bragdon Crossing	3200	MaineDOT	89.9	04/04/2013
Hilton	6216	Wells	38.5	01/13/2014
Sherburne	6122	MaineDOT	99.0	09/10/2013
Merriland Ridge Bridge	5338	MaineDOT	55.6	01/07/2013
B&M Railroad Overpass	5337	Railroad	-1.0	10/17/2013
Lewis West	3916	Wells	48.1	01/22/2014
Charles West	3915	Wells	73.3	01/21/2014
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Wells Interchange SB	1480	Maine Turnpike Authority	94.5	05/09/2012
High Pine Crossing	3199	MaineDOT	85.1	01/07/2013
Island Ledge Road	3175	MaineDOT	80.1	01/28/2014
Branch Brook	3091	MaineDOT	79.1	01/31/2014
Pumping Station	2693	MaineDOT	64.9	01/22/2014
Little River	2468	MaineDOT	85.8	11/01/2012
Edward Hill	2263	MaineDOT	72.2	10/10/2012
Capell	2126	MaineDOT	74.0	10/17/2013
Merriland River	6492	Wells	86.9	01/22/2014
Bourne Avenue	3765	MaineDOT	80.8	12/09/2013



Maine DOT Map










Map Generated on Wednesday, October 01, 2014 10:11:15 AM

Map Scale 1:86606

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Federal Functional Classification

-  *Principal Arterial Interstate*
-  *Principal Arterial / Other Freeway*
-  *Other Principal Arterial*
-  *Minor Arterial*
-  *Major/Urban Collector*
-  *Minor Collector*
-  *Local Road*

MaineDOT Regions



State Compact Area



Water Bodies



Wetlands

Traffic Counts: 2007, 2010, 2013									
	2007	2010	2013	2007-2013		2010-2013			
				Change	Percent	Change	Percent		
US 1/SR 9 NE/O Harbor Road	--	12,460	12,960	--	--	500	3.86%		
US 1 N/O Mile Road	--	20,120	17,070	--	--	-3,050	-17.87%		
US 1 S/O Mile Road	--	17,970	15,360	--	--	-2,610	-16.99%		
US 1 S/O Tatnic Road at Bridge #2239 and townline	14910	--	13,370	(1,540)	-12%	--	--		
SR 9A (Branch Road) SW/O Meeting House Road	1920	1,920	2,140	220	10%	220	10.28%		
SR 9B (Littlefield Road) SE/O SR 9 (North Berwick Road)	2600	2,540	3,090	490	16%	550	17.80%		
SR 9/109 (Sanford Road) E/O Chapel Road	--	--	9,420	--	--	--	--		
SR 9/109 (Sanford Road) W/O Chapel Road	--	--	14,970	--	--	--	--		
SR 9 (North Berwick Road) E/O SR 9B (Littlefield Road)	6120	6,080	6,360	240	4%	280	4.40%		
SR 9 (North Berwick Road) W/O SR 9B (Littlefield Road)	7980	7,800	8,460	480	6%	660	7.80%		
SR 9 (North Berwick Road) SW/O SR 9/109 (Sanford Road)	6880	6,980	6,980	100	1%	0	0.00%		
Quarry Road SW/O Bald Hill Road	740	890	820	80	10%	-70	-8.54%		
SR 109 (Sanford Road) SE/O SR 9A (Branch Road)	9240	9,180	8,800	(440)	-5%	-380	-4.32%		
SR 109 (Sanford Road) NW/O SR 9A (Branch Road)	8190	7,770	8,490	300	4%	720	8.48%		
SR 109 (Sanford Road) SE/O Wire Road	7140	7,930	7,630	490	6%	-300	-3.93%		

DOWNEASTER WELLS BOARDINGS & ALIGHTINGS

ONS	2002	2003	2004	2005	2006	2007	2008	2009	2010	2011	2012	2013	2014
Jan	0	608	769	777	947	1,072	1,330	1,348	1,369	1,510	1,742	1,721	1,529
Feb	0	512	925	892	1,004	979	1,431	1,541	1,490	1,411	1,854	1,389	1,433
Mar	0	592	861	898	1,060	1,046	1,610	1,485	1,669	1,798	1,881	1,665	1,663
Apr	1,814	900	1,024	987	1,145	1,053	1,792	1,700	1,732	1,928	1,959	1,712	1,822
May	1,483	953	850	1,122	1,180	1,275	1,869	1,503	1,746	2,098	1,999	2,051	1,767
Jun	1,478	1,065	1,083	1,279	1,487	1,486	2,346	2,027	2,265	2,407	2,337	2,274	2,241
Jul	1,627	1,421	1,162	1,927	1,961	2,209	3,364	3,078	3,070	3,385	3,623	3,295	2,926
Aug	0	1,462	1,693	1,948	2,198	2,388	3,461	3,131	3,341	3,284	3,493	3,460	0
Sep	0	1,045	1,213	1,561	1,561	1,819	2,375	2,086	2,586	2,525	2,439	2,427	0
Oct	784	983	955	1,312	1,202	1,588	1,849	1,833	1,903	2,235	2,021	1,875	0
Nov	736	817	1,058	1,184	1,405	1,486	1,748	1,731	1,855	1,878	2,443	1,804	0
Dec	758	830	971	1,115	1,236	1,448	1,391	1,460	2,033	2,050	1,954	1,747	0
Total	8,680	11,188	12,564	15,002	16,386	17,849	24,566	22,923	25,059	26,509	27,745	25,420	13,381
Daily Avg	31	34	41	45	49	67	63	69	73	76	70	63	

OFFS	2002	2003	2004	2005	2006	2007	2008	2009	2010	2011	2012	2013	2014
Jan	0	782	765	704	888	1,005	1,312	1,295	1,316	1,444	1,694	1,680	1,473
Feb	0	660	941	811	1,031	939	1,444	1,402	1,475	1,456	1,828	1,445	1,453
Mar	0	741	838	868	1,059	1,086	1,588	1,455	1,587	1,766	1,883	1,787	1,745
Apr	2,294	990	1,039	1,001	1,140	1,007	1,859	1,812	1,718	2,054	2,009	1,659	1,844
May	1,808	986	904	1,109	1,212	1,361	1,884	1,635	1,856	2,243	2,140	2,180	1,908
Jun	1,847	1,323	1,128	1,332	1,704	1,651	2,424	2,117	2,148	2,474	2,604	2,426	2,407
Jul	2,009	1,592	938	1,943	1,981	2,302	3,433	3,141	3,134	3,608	3,670	3,311	3,043
Aug	0	1,882	1,659	1,808	2,216	2,454	3,622	3,146	3,445	3,305	3,720	3,712	0
Sep	0	1,133	1,171	1,584	1,523	1,769	2,292	2,188	2,534	2,516	2,409	2,265	0
Oct	1,087	1,039	1,024	1,304	1,159	1,539	1,972	1,771	1,935	2,229	2,149	1,966	0
Nov	937	902	969	1,158	1,497	1,488	1,775	1,735	1,843	1,928	1,943	2,087	0
Dec	929	891	924	1,143	1,281	1,467	1,352	1,462	1,946	1,898	1,900	1,692	0
Total	10,911	12,921	12,300	14,765	16,691	18,048	24,957	23,159	24,937	26,921	27,949	26,210	13,873
Daily Avg	35	34	34	40	46	49	68	63	68	74	76	72	65

OFFS	2002	2003	2004	2005	2006	2007	2008	2009	2010	2011	2012	2013	2014
Jan	0	1,390	1,534	1,481	1,835	2,077	2,642	2,643	2,685	2,954	3,436	3,401	3,002
Feb	0	1,172	1,866	1,703	2,035	1,918	2,875	2,943	2,965	2,867	3,682	2,834	2,886
Mar	0	1,333	1,699	1,766	2,119	2,132	3,198	2,940	3,256	3,564	3,764	3,452	3,408
Apr	4,108	1,890	2,063	1,988	2,285	2,060	3,651	3,512	3,450	3,982	3,968	3,371	3,666
May	3,291	1,939	1,754	2,231	2,392	2,636	3,753	3,138	3,602	4,341	4,139	4,231	3,675
Jun	3,325	2,388	2,211	2,611	3,191	3,117	4,770	4,144	4,413	4,881	4,941	4,700	4,648
Jul	3,636	3,013	2,100	3,870	3,942	4,511	6,797	6,219	6,204	6,993	7,293	6,606	5,969
Aug	0	3,344	3,352	3,756	4,414	4,842	7,083	6,277	6,786	6,589	7,213	7,172	0
Sep	0	2,178	2,384	3,145	3,084	3,588	4,667	4,274	5,120	5,041	4,848	4,692	0
Oct	1,871	2,022	1,979	2,616	2,361	3,127	3,821	3,604	3,838	4,464	4,170	3,841	0
Nov	1,673	1,719	2,027	2,342	2,902	2,974	3,523	3,466	3,698	3,806	4,386	3,891	0
Dec	1,687	1,721	1,895	2,258	2,517	2,915	2,743	2,922	3,979	3,948	3,854	3,439	0
Total	19,591	24,109	24,864	29,767	33,077	35,897	49,523	46,082	49,996	53,430	55,694	51,630	27,254
Daily Avg	0	66	68	82	91	98	135	126	137	146	152	141	129

